

HOPKINS PLANNING & ZONING COMMISSION

AGENDA

Tuesday, March 22, 2022

6:30 pm

**THIS AGENDA IS SUBJECT TO CHANGE
UNTIL THE START OF THE PLANNING & ZONING COMMISSION MEETING**

- I. **CALL TO ORDER**
- II. **ADOPT AGENDA**
- III. **COMMUNITY COMMENT**
- IV. **CONSENT AGENDA**
 1. Minutes of the January 25, 2022 Planning & Zoning Commission Meeting
 2. Minutes of the February 22, 2022 Planning & Zoning Commission Work Session
- V. **PUBLIC HEARINGS**
 1. 2022-06 Blake Road Station PUD Amendment and Phase B Site Plan Review
- VI. **OLD BUSINESS**
- VII. **NEW BUSINESS**
- VIII. **ANNOUNCEMENTS**
- IX. **ADJOURN**

HOPKINS PLANNING & ZONING COMMISSION

WORK SESSION

AGENDA

Tuesday, March 22, 2022

Meeting to begin immediately following the regular meeting

Hopkins City Hall – Council Chambers

- I. Planning & Zoning Commission Training
- II. Other
- III. Adjournment

**HOPKINS PLANNING & ZONING COMMISSION
REGULAR MEETING PROCEEDINGS
JANUARY 25, 2022**

CALL TO ORDER

Pursuant to due call and notice thereof a regular meeting of the Hopkins Planning & Zoning Commission was held by virtual means pursuant to Minnesota Statute 13D.021 in response to the COVID-19 emergency on Tuesday, January 25, 2022 at 6:30 p.m.

Chairperson Stiele called the meeting to order with Commission Members White, Dyrland, Terrill, Wright and Sedoff attending. Also present was City Planner Lindahl and Management Analyst Imihy Bean.

ADOPT AGENDA

Motion by Sedoff. **Second** by Wright.

Motion to Adopt the Agenda as presented.

Ayes: Sedoff, White, Wright, Dyrland, Stiele, Terrill

Nays: None. Motion carried.

COMMUNITY COMMENT

None.

CONSENT AGENDA

Motion by White. **Second** by Terrill.

Motion to Approve the Consent Agenda

1. Minutes of the November 23, 2021 Planning & Zoning Commission Meeting

Ayes: Terrill, White, Wright, Dyrland, Sedoff, Stiele

Nays: None. Motion carried.

PUBLIC HEARINGS

None.

OLD BUSINESS

None.

NEW BUSINESS

1. Zoning Equity Working Group Report

Management Analyst PeggySue Imihy Bean provided an overview of the work done by the zoning equity subcommittee and their recommendations for the zoning code update. The overview summarized each area of potential zoning reform and staff next steps. The areas of potential zoning reform included: bike and pedestrian infrastructure requirements; building materials; form-based code design; sustainable development and solar requirements; increased notification standards; streetscaping; landscaping

**HOPKINS PLANNING & ZONING COMMISSION
REGULAR MEETING PROCEEDINGS
JANUARY 25, 2022**

and lighting; accessory dwelling units; standards in single-family home neighborhoods; inclusionary zoning and inclusionary housing policy; and parking standards.

Commissioner Sedoff questioned how the City will ultimately use the recommendations from the zoning equity subcommittee. Planner Lindahl explained the overall process of the zoning code update and how the equity recommendations would be incorporated.

Commissioner White and Commissioner Sedoff voiced concern over allowing two-unit dwellings in additional zoning districts. Commissioner Sedoff requested further clarification on the language used to describe single family neighborhoods and how they are a direct result of exclusionary zoning practices. Analyst Imihy Bean provided background information on how zoning has historically been used as an exclusionary practice to exclude communities of color from purchasing homes through racially restrictive covenants and bank lending processes. Commissioner Wright commented on how these equity recommendations start the process of righting the damage of the past. After additional discussion between the Commissioners relating to racial equity and the history of exclusionary zoning and housing policies, Ms. Imihy Bean concluded her overview of the recommendations from the zoning equity subcommittee.

2. Planning & Zoning Commission 2022 Work Plan

City Planner Lindahl summarized the Advisory Board Handbook, which was developed as a resource to help guide the work of the City's boards and advisory commissions, and the Planning & Zoning Commission's 2022 Annual Work Plan. In 2022, the work plan will include review of the updated zoning ordinance, training opportunities and the process to work with the City Council to develop the 2023 work plan.

Discussion on the work plan included opportunities for additional education on zoning history and other relevant topics and adding items to the work plan.

Planner Lindahl stated that the next step is to present the work plan to the City Council and begin to implement the plan in the following months.

ANNOUNCEMENTS

1. Planning & Zoning Commission Training

Chairperson Stiele requested that staff provide the training video to the Commissioners to watch remotely outside of the meeting.

ADJOURNMENT

Motion by Dyrland. **Second** by Terrill.

Ayes: White, Wright, Dyrland, Sedoff, Terrill, Stiele

Nays: None. Motion carried.

The meeting adjourned at 8:30 p.m.

**HOPKINS PLANNING & ZONING COMMISSION
REGULAR MEETING PROCEEDINGS
JANUARY 25, 2022**

Respectfully submitted,
Courtney Pearsall, Administrative Assistant

ATTEST:

Samuel Stiele, Chair

**HOPKINS PLANNING & ZONING COMMISSION
WORK SESSION PROCEEDINGS
FEBRUARY 22, 2022**

CALL TO ORDER

Pursuant to due call and notice thereof a work session of the Hopkins Planning & Zoning Commission was held on Tuesday, February 22, 2022 at 6:30 p.m. in the Raspberry Room at City Hall, 1010 1st Street South.

Chairperson Stiele called the meeting to order with Commission Members White, Dyrland, and Wright attending. Also present was City Planner Lindahl.

PLANNING & ZONING COMMISSION TRAINING

The Planning & Zoning Commission held a work session for training on planning ethics. The Commission watched a video from planetizen.com narrated by faculty from the Michigan State University School of Planning, Design & Construction's Urban & Regional Planning Program.

The video covered various ethical topics including ethics and fairness, conflicts of interest, American Planning Association ethical principles in planning, and planning official conduct. During and after the video, the commission had general conversation about ethics and how they could incorporate the information in the video into their individual roles as Planning & Zoning Commissioners.

ADJOURNMENT

The meeting adjourned at 8:00 p.m.

Respectfully submitted,
Courtney Pearsall, Administrative Assistant

ATTEST:

Samuel Stiele, Chair

BACKGROUND

This Background section provides a history of existing site, project summary, and a brief summary of the review process for this development to date. More detailed information about the project, it’s phasing, design, streetscape and public realm, and retail components can be found in the applicant’s narrative (see attached).

Existing Site. The 6.4-acre subject property was re-platted as Lots 1, 2 & 3, Hopkins Commerce Addition in July of 1992. However, the buildings on each of these lots were built much earlier. Lot 1 (Site C) contains a 44,000 square foot building constructed in 1954 (Goodwill and vacant space). Lot 2 (Site A) contained a 25,000 square multitenant strip commercial building constructed in 1957. The building on Lot 2 has been cleared and construction on Site A began late last year. Lot 3 (Site B) contains a 26,000 square foot multitenant strip commercial building constructed in 1959. In total, the 3 multitenant strip commercial buildings contained approximately 95,000 square feet of neighborhood serving retail and service uses.

Project Summary. According to the applicant, the Blake Road Station project will redevelop 6.4 acres of land located at the intersection of Excelsior Boulevard and Blake Road into a multi-modal, transit-oriented development adjacent to the future Blake Road Station along the Metro Green Line Extension. Each of the 3 phases of this development are designed to build upon the improvements of the previous phase and are intended to create an increasingly connected, pedestrian friendly, and transit focused neighborhood. As detailed in the table below, overall the site will include 770 residential units and just over 12,000 square feet of retail space.

Blake Road Station Development Summary					
Site	Start	Commercial Space	Residential Units	Parcel Size	Density
A	December 2021	2,100	220	1.55 Acres	142 Units/Acre
B	Summer 2022	10,062	250	2.1 Acres	119 Units/Acre
C	2026-2031	0	300	2.76 Acres	109 Units/Acre
Total	N/A	12,162	770	6.4 Acres	120 Units/Acre

Review Process to Date. The review process for this project included both the City’s land use and zoning review and the required State of Minnesota environmental review processes. The City Council and Planning & Zoning Commission reviewed the concept plan for this development in June 2020 and were generally supportive of the design. As a concept plan review, no public hearing was required. However, because of the ongoing COVID-19 pandemic the City did require the applicant to hold a virtual neighborhood meeting to engage the community about their redevelopment plans. Community comments from the concept plan neighborhood meeting focused on the following topics:

- Business displacement and a need to incorporate affordable neighborhood serving commercial uses.
- Need for affordable and larger apartment units with 3-4 bedrooms.
- The importance of pedestrian scaled architecture.
- The importance of pedestrian and bicycle access to and through the site.

In response to these comments, the applicant made an effort to improve communication with existing businesses about the timeline for the redevelopment, provide reduced rents and allow

additional time for businesses to relocate.

After the concept plan review, the project went through the state required environmental review process in 2020. Based on the size and scope of this redevelopment, the project was required to go through the environmental assessment worksheet (EAW) process. The EAW process included a parking study and a 30-day comment period that resulted in comments from 5 state agencies, Hennepin County, the Hopkins Housing Team and 1 Hopkins resident. The Planning & Zoning Commission reviewed the EAW on October 14th and recommended City Council approval. The City Council then reviewed the EAW on October 20th and made a negative declaration of need regarding the EAW clearing the way for the developer to go through the City's development review process.

Following the EAW process, the applicant submitted development applications for the overall Blake Road Station development and the first phase of this project in June of 2021. These applications include rezoning, planned unit development (PUD) and site plan review for the first phase of the development (Site A). The review process for these applications included an in-person neighborhood meeting on July 14, a public hearing before the Planning & Zoning Commission on July 27 and review and action by the City Council during their August 2 and August 17, 2021 meetings. During the development review process, public comment included the need for affordable housing and commercial space, pedestrian and bicycle improvements, displacement of existing businesses, environmental concerns and the need for an equitable development review of the proposal.

Neighborhood Meeting. The current PUD amendment and site plan review applications require the applicant to host a neighborhood meeting. That meeting took place on March 2, 2022 from 6:00 – 7:30 PM at 1002 2nd Street Northeast (43 Hoops). Neighborhood meetings must be hosted by the developer and require them to engage with the surrounding neighborhood to explain their project, answer questions and take any comments. In addition to the in-person meeting, the applicant was also required to set up a webpage to allow online comments. According to the applicant, they did not receive any online comments and the overall questions or concerns from the in-person meeting included the items list below. The applicant's responses to these items are detailed in their meeting notes (see attached).

- Affordable housing
- Inquiries from current and prospective tenants about retail space
- Project timeline
- Pedestrian connections and improvements.

Community Comment. Under state law, these applications require a public hearing. Accordingly, the City published notice of this request and public hearing in the Sun Sailor, mailed notices directly to all property owners and existing tenants within 500' of the subject property, posted a sign on the subject property to inform the general public of the proposed project and added information about the project and staff and developer contact information to the City's website.

The notice directed all interested parties to send questions or comments to City Planner Jason Lindahl by mail, phone or email or to attend the public hearing where they could learn about the request, ask questions and provide feedback. As of the writing of this report, the City had

received no comments or questions regarding this request. During the public hearing, staff will provide an update on any public comments received prior to the Planning & Zoning Commission meeting.

PLANNED UNIT DEVELOPMENT AMENDMENT

The applicant received approval of the Blake Road Station PUD and associated Master Plan in August of 2021. The PUD establishes the Mixed Use zoning district and its development regulations as the base zone and standards for review. Overall, the PUD entitled lot configuration, access and private road configuration, and building height for all 3 sites as well as the setback standards for Site A.

Review of Site B requires separate site plan review approval based on the entitlements established in the PUD Master Plan noted above and the specific zoning standards of the Mixed Use district. Deviations from the Mixed Use standards, other than those noted above require an amendment to the existing Blake Road Station Planned Unit Development (PUD) agreement.

As you may recall, the purpose of a PUD is to allow flexibility from traditional development standards in return for a higher quality development. Typically, the City looks for a developer to exceed other zoning standards, building code requirements or meet other goals of the Comprehensive Plan. In exchange for the flexibility offered by the planned unit development, the applicant is expected to detail how they intend to provide a higher quality development or meet other City goals.

The requested deviations and items offered by the developer in exchange for the requested deviations for Site B are compared in the table below. Each of the requested deviations is compared with the applicable Mixed Use zoning district standards in the Site Plan review section below.

PUD Comparison for Blake Road Station Site B	
Requested Deviations	Items Offered to Support the PUD
Building Height	Transit Supportive Density
Floor to Area Ratio	Linear Park
Setbacks on all 4 Sides of the Building	20 EV Charging Stations
Parking Stall Dimensions	
Exterior Building Materials	
Transparency & Fenestration	
Façade Articulation	

When weighing a PUD request, the City should consider if the proposed deviations and items offered by the developer create a higher quality development, if there is a reasonable trade-off between these items and if the items offered by the developer help to mitigate the requested deviations.

Future review of Site C will required separate site plan review approval based on the entitlements established in the PUD Master Plan and the specific zoning standards of the Mixed Use district. Deviations from the Mixed Use standards for Sites C other than those specifically noted in the PUD agreement will require an amendment to the existing Blake Road Station Planned Unit Development (PUD) agreement.

It should also be noted that the applicant's narrative for the original PUD states their intent to subdivide these properties as part of future development of Site C. Because this project is along a county road, both the City of Hopkins and Hennepin County will have plat review authority. With any subdivision, the City has the ability to collect park dedication in the form of land or a fee-in-lieu. The original PUD established the park dedication rates in place in 2021 which are \$3,000 per unit for multiple family residential and five (5) percent of the fair market value of the commercial land as estimated by the county assessor.

SITE PLAN REVIEW FOR SITE B

Standards for reviewing site plan applications are detailed in Article IV of the City Code. This section establishes the site plan review procedure and provides regulations pertaining to the enforcement of site design standards. These procedures are established to promote high quality development that ensures the long-term stability of residential neighborhoods and enhances the built and natural environments within the City as new development and redevelopment activities occur. Specifically in this case, site plan review is required for the new mixed-use building. Staff has reviewed the proposed development against the zoning regulations for the Mixed Use district and the standards established in the Blake Road Station PUD. Staff's review and findings for each of these regulations are detailed below.

Land Use & Zoning. The subject property is guided as Activity Center in the 2040 Comprehensive Plan. The Activity Centers future land use category surrounds and supports the planned Blake Road and Shady Oak light rail stations along the Metro Green Line Extension. These areas are planned to include moderate to high density mixed use development designed to complement and enhance the existing development pattern in these areas and support the public investment in transit. Densities in these areas will typically range from 20-60 units per acre, with 75-150 units per acre within ¼ mile of an LRT station platform. The subject property is within the ¼ mile radius of the future Blake Road Station and has a density of 119 Units/Acre. As noted above, the site is also zoned Mixed Use with a planned unit development which is consistent with the comprehensive plan designation.

Height. The applicant's plans call for the building on Site B to be a total of 7 stories which is consistent with the height standard established for Site A, B and C under the Blake Road Station PUD. In total, the building will have one level of parking below grade, two levels of concrete construction for commercial and amenity spaces and additional parking, and five stories of wood-framed construction for residential apartment units. As a mixed-use building, the base Mixed-Use zoning would allow the building to be up to six stories in height. However, the PUD allowed for one additional story in exchange for the development more closely meeting the 2040 comprehensive plan density goals around light rail stations.

Floor to Area Ratio (FAR). As designed, the Floor to Area Ratio (FAR) for Site B fails to meet the standard of the Mixed Use district. FAR is a measurement of density calculated by dividing the floor area of a building by the lot area of the parcel on which the building is located. In the Blake Road Station Area, the FAR standard is a minimum of 2 and a maximum of 3 for residential buildings and a minimum of 3 and a maximum of 5 for mixed use buildings. The building on Site A is considered a mixed-use building because it includes commercial space. With a proposed FAR of 2.8, the building on Site B does not conform to the minimum 3.0 FAR standard. However, given the overall design of the site, the proposed 2.8 FAR is acceptable under the PUD.

Setbacks. As designed, the building on Site B does not meet the setback standards for the Mixed Use district. The table below compares the required and proposed setbacks for Site B. Overall, the proposed setbacks are less than required on the north, south and east sides of the building but greater than required on the west side of the building. This design results in additional space for a linear parking along the west side of the site which results in a more pedestrian and transit focused layout and is acceptable under the overall planned unit development.

Setback Review for Blake Road Station – Site A			
Setback	Standard	Proposed	Status
Front (South)	15' – 25'	10'-8" to 18'-2"	Non-Conforming (-4'-4" to -6'-10")
Front (East)	15' – 25'	9'-8" to 28'-5"	Non-Conforming (-5'-4" to +3'-5")
Front (west)	15' – 25'	32'-8" to 49'-10"	Non-Conforming (+22'-8" to +24'-10")
Rear (North)	10'	5'-3" to 20'-7"	Non-Conforming (-4'-9" to +10'-7")

Note: Setbacks are measured from the foundation; however, it should be noted that stories 3-7 along the south side of the building are recessed which results in great visible setback for these upper floors.

Off-Street Parking & Travel Demand Management. The applicant conducted a traffic impact analysis as part of the concept plan and environmental review processes. Overall, the study confirmed the City’s off-street parking requirements of the Mixed Use district and the number of parking spaces proposed by the developer were adequate for all 3 buildings.

A specific review of Site B finds the proposed off-street parking meets the requirements of the Mixed Use district. These standards require residential uses provide a minimum of 1 and a maximum of 1.5 enclosed parking stalls per unit and 1 guest stall per 15 units. Using these standards, the 250 unit building proposed for Site B is required to provide at least 267 off-street parking stalls and a total of 339 are proposed.

Of the 339 total proposed stalls, 297 are reserved for residential use while 42 are designated to serve the commercial uses. Under this parking division, the site exceeds the minimum amount of required residential parking by 30 stalls but is 8 stalls under the minimum commercial parking standard of 1 stall per 200 square feet. Therefore, staff recommends a condition of approval require the applicant to reallocate the parking so that 289 stalls are assigned to residential and 50 are assigned to commercial uses.

The recommended allocation of parking results in a parking ratio of 1.15 stalls per unit for residential and 1 stall per 200 square feet for commercial. By comparison, Site A has 256 total stalls with a parking ratio of 1.16 stalls per unit.

Bicycle Parking. According to the applicant’s narrative, bicycle parking on Site B exceeds the minimum requirements of the Mixed Use district. For multifamily residential, the Mixed Use district requires 1 long-term space per 2 units and 1 short-term space per 20 units or a total of 138 bicycle parking spaces. According to the applicant, there will be 150 bicycle parking stalls for residential use within the building. However, the location of these stalls is not illustrated on their plans. Staff recommends that a condition of approval require the applicant to revise their plan to show at least 138 long-term bicycle parking stalls within the building and at least 13 short-term stalls outside the building.

The Mixed Use district also requires bicycle parking for the commercial space. These standards require at least 0.5 long-term spaces per employee and 0.5 short-term spaces per 1,000 square feet of net building area. According to the applicant’s narrative, to meet these requirements Site B will have 20 bicycle racks located within the right-of-way adjacent to the building’s primary entrance. However, the required 5 long-term space must be located within the building. Staff recommends that a condition of approval require the applicant to revise their plan to provide at least 5 long-term commercial bicycle parking stalls inside the building in close proximity to the commercial spaces. The balance of the 15 commercial bicycle parking stalls may be located outside the building close to the entrances to the commercial space. Any bicycle parking located within the public right-of-way must be approved by the City and or Hennepin County.

Shadow Study. The Mixed Use district requires a shadow study for all buildings four stories or higher and the applicant has included this study with their plans. According to the Mixed Use district standards, the shadow study must indicate the shadows cast at the shortest and longest days of the year. Impacts of a shadow on the surrounding property may be a reason to lower and/or adjust the location or height of buildings. Staff review of the shadow study finds little to no impact on adjacent properties even with the additional height to 7-stories allowed through the PUD.

Exterior. Exterior materials proposed for Site B do not meet the standards of the Mixed Use district. The Mixed Use district requires the primary exterior treatment of walls facing a public right-of-way or parking lot on a structure shall be brick, cast concrete, stone, marble or other material similar in appearance and durability. Regular or decorative concrete block, float finish stucco, EIFS-type stucco, cementitious fiberboard, or wood clapboard may be used on the front façade as a secondary treatment or trim but shall not be a primary exterior treatment of a wall facing a public right-of-way. Staff interprets primary to be at least 65 percent and secondary to not exceed 35 percent of the side of a building. The proposed exterior materials for the building on Site B are summarized in the table below.

Exterior Materials for Blake Road Station Site B				
Material	North	South	East	West
Brick Veneer	32%	32%	15%	44%
Metal Panel	7%	8%	15%	12%
Metal Tile	36%	37%	32%	31%
Fiber Cement Lap Siding	25%	23%	38%	14%
Total	100%	100%	100%	101%???

Of the proposed exterior materials detailed above, only the brick qualifies as a primary material and it covers 32% of the north and south, 15% of the east and 44% of the west sides of the building on Site B. While these are significantly less than the requirement, the applicant notes that the brick material is concentrated on the first two levels of all sides of the building to enhance the ground level pedestrian experience. This design and mix of materials for the building on Site B is acceptable under the overall planned unit development.

Building Orientation. The building on Site B conforms to the building orientation standards of the Mixed Use district. Buildings within the Mixed Use district must be oriented toward the pedestrian by providing a direct link between each building and the pedestrian walking system, with emphasis on directing people to a transit station.

The applicant's plans for Site B show the building will have pedestrian connections to the existing sidewalk along both Blake Road and Excelsior Boulevard that will eventually connect with the sidewalks and streets planned for overall site and the City's sidewalk and trail system. Staff recommends a condition of approval require the applicant to revise their plans to include architectural features that will make the building entrance along Blake Road more noticeable to pedestrians.

Transparency & Fenestration. The building on Site B does not meet the transparency & fenestration standards of the Mixed Use district. The Mixed Use district requires a minimum of 60 percent to a maximum of 75 percent of the front street-facing facade between two feet and eight feet in height must comprise clear windows that allow views of indoor nonresidential space or product display area. And the building must have at least 30 percent fenestration for windows above the first floor for all sides of the building.

The applicant's narrative and plans only provide transparency information for the north and west side of the building along the first floor and those percentages do not meet the minimum transparency standards. Staff recommends that a condition of approval require the applicant to revise their plan to provide transparency on the first floor of at least 60 percent along the south, east and west sides of the building and at least 50 percent along the north side of the building. The applicant shall also submit information demonstrating at least 30 percent fenestration for windows above the first floor on all sides of the building.

Facade Articulation. The building on Site B does not meet the facade articulation standards of the Mixed Use district. These standards require the primary street side facade of a building shall not consist of unarticulated blank walls, flat front facades or an unbroken series of garage doors. The front of a building shall be broken up into individual bays of a minimum of 25 feet and maximum of 40 feet wide. While of the building's architectural elements (window, balconies and materials) provide some break-up of the elevation, there is no variation in the roofline and variation in the plain of the elevation exceed the 25-foot minimum. Overall, this design is acceptable under the PUD provided the applicant revise their plans to include a building cap architectural feature (parapet with a horizontal shadow line or lintel).

Sidewalks. Site B meets the minimum sidewalk standards of the Mixed Use district. The Mixed Use district requires a minimum 5-foot wide sidewalk along the frontage of all public streets and within and along the frontage of all new development or redevelopment. The applicant's plans illustrate sidewalks of at least 5 feet along all four sides of the building; however, the sidewalk along the north side of the building passes through a corridor that is just over 5 feet. As a result, staff recommends a condition of approval require the applicant provide enhanced architecture on the first two stories along the north side of the building to improve the pedestrian experience through this corridor (see transparency section above).

Pedestrian/Streetscapes. The applicant's plans appear to meet the pedestrian and streetscape standards of the Mixed Use district. The Mixed Use district requires pedestrian streetscape improvements along all sidewalks. Pedestrian improvements of at least 25 percent of the landscaping budget shall be included in the development. These improvements shall create a high quality pedestrian experience through the provision of benches, planters, drinking fountains, waste containers, median landscaping and the like. Pedestrian-scale light fixtures that

shine downward on the sidewalks and walkways shall be no greater than 12 feet in height and must be provided along all sidewalks and walkways to provide ample lighting during nighttime hours for employees, residents, and customers. Staff recommends a condition of approval require the applicant to provide more detailed streetscape information for street furniture and light (see also landscaping and exterior lighting sections below).

Landscaping. The applicant's landscape plan for Site B exceeds the landscaping standards for the Mixed Use district. According to the applicant's estimates, this project is required to provide \$103,000 in landscape improvements with at least 25% allocated to pedestrian/streetscape improvements (see section above). In this case, the applicant's plans include \$286,000 of total landscaping improvements with \$188,000 of plantings, fencing and irrigation and \$98,000 (or 34%) in pedestrian/streetscape improvements. Staff recommends a condition of approval require the applicant to provide a landscape and pedestrian/streetscape letter of credit in a form acceptable to the City Attorney for 125% of the value of all improvements. The City will hold the letter of credit until all pedestrian/streetscape improvements have been installed and all plantings have survived at least one growing season.

Signs, Canopies and Awnings. The plans for Site B do not include a specific sign plan. However, the plans do include general information about a monument style sign in the southeast corner of the site adjacent to the Blake Road and Excelsior Boulevard intersection. All signs must be approved through a separate administrative permit subject to conformance with the standards of the Mixed Use district and sign regulations contained in Article XXI - Signs. It should be noted that the original PUD allowed for a specific ground sign located at the southwest corner of Site B. This item is addressed in the original PUD and is in addition the future sign proposed for the southwest corner of Site B.

Trash Enclosure. The applicant plans to store trash containers within the underground parking garage. The applicant shall designate a specific location within the Level 1 parking garage and an on-site location for trash pickup. Trash pick-up shall not occur within the public right-of-way or any street surface.

Exterior Lighting. The applicant's plans for Site B include a photometric site plan but does not include detailed light fixture information. The Mixed-Use district standards require pedestrian-scale light fixtures that shine downward on the sidewalks and walkways shall be no greater than 12 feet in height and must be provided along all sidewalks and walkways to provide ample lighting during nighttime hours for employees, residents, and customers.

Staff recommends a condition of approval require the applicant submit and receive approval of a detailed lighting and photometric plan for Site B prior to action on this item by the City Council. This plan shall include both pedestrian-scale street lights (no greater than 12' in height) as well as all building exterior lighting, light fixture spec sheets and photometric data demonstrating equal to or less than 1.0 lumens at any non-residential property line or 0.5 lumens at any residential property line.

Stormwater Treatment. The applicant's narrative includes stormwater treatment information that indicates it exceeds both the City's and Minnehaha Creek Watershed District's (MCWD) standards. According to the applicant, runoff from Site B will be conveyed to the future 325 Blake Road regional stormwater treatment facilities via the Powell Road diversion. More

detailed information about the applicant's stormwater treatment plans can be found in the attached project narrative. Staff recommends a condition of approval require the applicant to receive all necessary approvals from the City Engineer and the Minnehaha Creek Watershed District prior to issuance of a building permit.

ENGINEERING REVIEW

The Engineering Department offers the following comments.

- Conformance with all applicable conditions of the PUD Master Plan approval.
- Conformance with all requirements of the Traffic Impact Study.
- Storm Sewer
 - Watershed review and approval required
 - All underground detention and newly installed infrastructure will be private, and will be the responsibility of the owner/developer for all future maintenance.
- Water/Sewer
 - All newly installed infrastructure will be private, and will be the responsibility for the owner/developer for all future maintenance. This includes hydrants, valves, and service connections.
 - ROW permit will be required for any work in the public ROW. This includes Hennepin County and Hopkins.
 - There are no local SAC or WAC fees. Met Council SAC fees may apply.
- The monumentation at Blake and Excelsior may be removed.
- The City is only responsible for snow removal for sidewalk and trails within the public ROW. The owner is responsible for all other snow removal. The developer may remove snow from the public ROW.
- Lighting with the ROW shall match adjacent corridor lighting.
- Irrigation shall be provided in all ROW boulevards. Coordinate location and type with Hopkins Public Works.

ALTERNATIVES

1. **Recommend approval of the planned unit development (PUD) amendment & site plan for Blake Road Station Site B.** By recommending approval of these applications, the City Council will consider a recommendation of approval.
2. **Recommend denial of the planned unit development (PUD) amendment & site plan for Blake Road Station Site B.** By recommending denial of these applications, the City Council will consider a recommendation of denial. Should the Planning & Zoning Commission consider this option, it must also identify specific findings that support this alternative.
3. **Continue for further information.** If the Planning & Zoning Commission find that further information is needed, these items should be continued.

Site Location Map for Blake Road Station



PROJECT OVERVIEW

The Blake Road Station project is the redevelopment of 7.4 acres of land located at the intersection of Excelsior Boulevard and Blake Road South in Hopkins, MN. Trilogy Real Estate Group has been working with the City of Hopkins to develop the site in a three-phase, multi-modal transit-oriented development adjacent to the Blake Road Southwest LRT Station. The project received PUD and Phase A site plan approval in August of 2021, allowing for construction to begin on the first building in December of 2021. The second phase, which is the subject of the current site plan approval request, is situated on the corner of Excelsior and Blake Rd and will serve as a welcoming gateway to the project and City of Hopkins. When the third phase is completed, the development will be an anchor for the new transit station and will advance long-standing City goals outlined in numerous area planning documents, including the *East Hopkins Land Use & Market Study* (2003), *Blake Road Corridor Small Area Plan* (2009), *Blake Road Corridor Study* (2015) and the *Blake Road LRT Station Area Development Guidelines* (2015), as discussed in more detail in the latter part of this narrative.

As early as 2003, the *East Hopkins Land Use & Market Study* confirmed that redevelopment within the City's east end along Excelsior Boulevard and Blake Road would require a significant catalyst to help overcome certain existing barriers and obstacles to market delivery of such redevelopment. The applicant believes that the high-quality redevelopment of the project site and creation of a new transit-oriented node with significant residential density will provide such a catalyst. Each phase will build upon the improvements of the previous phase and will ultimately result in an increasingly connected, pedestrian friendly, and transit-focused community. The project will add to the mix of housing choices for area residents, providing a new opportunity to live immediately adjacent to the light rail transit station.

STREETScape AND PUBLIC REALM

The development team has approached this site as a unique opportunity to complement the transformative power of the Southwest LRT line. Each phase of the Blake Road Station project defines and beautifies the streetscape while complementing the crucial transit functions in and around the site. Currently, two of the three parcels on the site are comprised of older, stressed pavement with ill-defined access and a plethora of surface parking stalls. The Planned Unit Development that was previously entitled will transform the site and emphasizes a neighborhood scale of connected streets, with traffic calming measures and pedestrian features that will significantly improve the aesthetic of the gateway to the City of Hopkins.

This is the second of three parts of the projects vision that includes extensive pedestrian improvements for the area including sidewalk connections through the site with boulevards and a rhythm of deciduous street trees will provide a human-scale canopy year-round. The trees' summer shade reduces the heat island effect, creating a comfortable microclimate when needed most. Both the trees and parallel parking zones will help to create visible friction that will successfully reduce driving speeds to provide a safer environment for people, bikers, and drivers. The pedestrian zone along Excelsior Boulevard and Blake Road expands to a width suitable for public street amenities including benches, bike racks, street lighting and planting beds.

Phase B of the development will begin to define a new north-south street that divides the sites in two

and will connect to the SWLRT station. This phase creates the spine between the first phase from which stems entries, active uses, and a newly created linear pocket park. Adjacent to the future use parcel that sits immediately to the north of this phase of the project, the pocket park will feature seating, natural play features, landscaping, and a pedestrian walk that offers a connection from Blake Road to the interior of the development. The pedestrian connection will provide flexibility to adapt to opportunities that may develop on the future use parcel that is not currently controlled by the applicant.

BUILDING DESIGN

Blake Road Station will provide creative, mid-rise residential buildings that maximize density while emphasizing green space and public realm at the street level. The proposed style of construction for Buildings B is five levels of wood framing over two levels of concrete podium and a below-grade level of parking; similar to Building A. Trilogy envisions a community that values leisure and comfort with the ease of urban connection. The exterior building design draws on these qualities of being a transition from an urban to naturalistic landscape. The architecture offers a refined exterior material palette that reinforces this site's potential as a catalyst for beautiful design and smart growth in the Blake Station area of Hopkins. The palette, forms, colors, textures, and details create a coherent whole that is energizing and enduring.

The design of Site B builds on the design language established by Site A. Where Site A has a warm, neutral tones to evoke a feeling of rural comfort, Site B uses similar materials and massing but in a higher contrast color scheme that is dressier and more formal, better suited to its location and role as a gateway building. While the forms of Site B are simple and peacefully resolved they are meant to have more of a "signature" presence both for the development and for the city of Hopkins.

At street level, the sidewalk experience at the base of Site B is nearly identical to that of Site A. This is intentional so that the campus will have a sense of place with small differences that can be explored. The articulation in the masonry of Site B is also slightly more animated to enliven the pocket park. Site B has much more commercial square footage. The commercial sidewalk experience is metal clad. It has a clean and modern look, clearly differentiated from the residential and amenity areas at street level, while also maintaining specific historic references to a cornice and traditional façade signage. Blade signs also neatly slot into the elegant panel system used to define the retail facades. Building lighting is utilized to enhance the sidewalk experience throughout.

At the upper levels, all facades are clad with durable materials. Masonry and medium gauge metals and composite metal panels along with quality windows comprise more than eighty percent of the façade surfaces on the primary sides of the building along Blake and Excelsior. The primary sides as well as the west side of the building are articulated with separate clearly defined masses, each with its own carefully composed façade whether the function within the building is residential, parking, retail, or private amenity. The projecting masses are wrapped consistently on all three sides, expressing how the building is organized into separate groupings in a beautiful and balanced architecture while also saying something about how each of the individual homes within are configured. Fiber cement lap siding is shown in recesses in order to visually separate these areas from the main facades' more prominent architectural elements. As with Site A, the metal portions of the cladding on Site B will also have an attractive luster. Metallic luster allows the building to show its personality by dressing itself in the colors of its surroundings, most beautiful at sunrise and sunset but also softly echoing the glow of the carefully designed and manicured pedestrian realm at ground level, reflecting its current lighting configuration and its active uses.

Amenity courtyards, balconies, and terraces extend living spaces to the outdoors while improved sidewalks and connections encourage active lifestyles for residents. Ground floor retail space will enhance the streetscape experience and provide an opportunity for retailers to have an intimate connection with residents, LRT riders, and the surrounding Hopkins community. With proximity to the Cedar Lake Trail, each building will supply ample storage and access to bicycle repair facilities to encourage bicycle use for recreation as well as daily commuting.

Similar to Site A, these state-of-the-art dwelling units will support the residents' sustainable living experience by providing urban housing that incorporates energy-efficient appliances, low-flow water fixtures, low-VOC paints, building-wide recycling practices and encouraging car-free living. The buildings will be designed to incorporate assemblies that ensure high-quality acoustical performance between units (wall and floor assemblies).

DEVELOPMENT PHASING

Commencement of construction of the various phases is dependent upon market conditions and existing lease agreements. Subject to those conditions, the development team currently anticipates the following construction start dates for the various phases:

Building A: Under Construction

Building B: Summer 2022

Building C: Spring 2026 – 2031 (pending existing lease agreements)

CITY APPLICATIONS/APPROVALS

- Previous Approvals
 - Zoning Application: Requesting rezoning from B-4 (Neighborhood Commercial) and I-1 (Industrial) to Mixed Use District
 - Planned Unit Development (PUD) Submittal (for the Overall Site)
 - Site Plan Approval for Site A (Site Plan Review Application)
 - Travel Demand Management Study approved by the City of Hopkins and Hennepin County
 - Execution of a Planned Unit Development Agreement
- Site Plan Approval for Site B (Site Plan Review Application)
- The development plan may necessitate re-platting of the land in future phase C
- Approvals from the Minnehaha Creek Watershed District
- Amendment to the Planned Unit Development Agreement

PROPOSED DEVIATIONS FROM MIXED USE DISTRICT STANDARDS

Strict adherence to the Mixed-Use District standards and requirements is not required in this case to satisfy the intent of the City’s planned unit development provisions and the proposed deviations from those standards will not prejudice the health, safety or welfare of the residents of the development, the surrounding area or the city as a whole. The proposed deviations from Mixed Use District standards for the Blake Road Station Site B are summarized in the following table:

Requested Deviations		
Category	Mixed-Use Standards	Proposed for Site B
Building Height	6 stories	7 stories granted with previous PUD approval
Floor Area Ratio	3.0	2.8
South Setback	15’ – 25’	10’-8” to 18’-2” on Levels 1 and 2 25’-1” to 30’-8” on Levels 3 thru 7
West Setback	15’ – 25’	32’-8” to 49’-10”
East Setback	15’ – 25’	9’-8” to 28’-5”
North Setback	10’	5’-3” to 20’-7 on Levels 1 and 2 11’-1” to 20’-4” on Level 3 thru 7
Parking Stall Dimensions	9 ft x 20 ft	9 ft x 18 ft
Primary Exterior Building Materials	The zoning material guideline does not identify the metal panel tile as a primary or secondary material	High-quality, primary materials of brick and glazing are proposed at Levels 1 and 2 of Building B. The applicant proposes the metal tile material as an equally durable and attractive option to other primary materials.
Exterior Building Material Percentage	20% maximum for secondary materials such as fiber cement siding	East elevation – Fiber cement siding, a secondary material, covers about 24% of the east elevation. Requested deviation to allow up to 30% for secondary materials on the east elevation.
Building Transparency	60% transparency between 2 ft and 8 ft	West elevation: 30% tinted or opaque glazing and 20% recessed brick is proposed between 2 and 8 ft.*
Signage	One monument sign permitted for each multi-tenant building provided the surface area of the sign does not exceed 2 sq ft per front foot of lot. No sign shall be over 150 sq ft, 20 feet in height and have a setback in no case less than 20 ft from the property lines.	Approved as part of the PUD, a 3 ft x 6 ft monument sign at the southwest corner of Site B is proposed as a marker for Goodwill.

SUPPORT FOR PROPOSED DEVIATIONS

The applicant believes the development, as proposed with deviations from zoning code ordinances, is a high-quality proposal for the City of Hopkins and better aligns with overall Comprehensive Plan goals. To ensure a high-quality development, the applicant is proposing the following site enhancements in support of the proposed deviations:

Enhanced Pedestrian Scale Architecture

Primary building materials of brick, metal and glass are focused at the first two levels of the street-facing elevations. Other building elements such as canopies, building mounted light sconces, a trellis over the outdoor seating area and recessed balconies enhance the street presence and pedestrian experience at the ground floor. Stepping back the building at the third level along Blake and Excelsior creates a 2-story expression at focused pedestrian areas to further emphasize the human scale. Finally, the selected metal panel tile is both modern and traditional, with a shingle shape that creates a more residential scale at the upper-level elevations.

Site signage will be constructed with materials that match the architectural style and materials of the building. The base of the primary monument sign for the development will be surrounded by landscaped planting beds and enhanced with seating opportunities. The development team is open to working out a solution for public art at this intersection.

Active Uses and Building Transparency

With effectively three front yards on Site B, the pedestrian scale design and site programming ensures articulation and interest on all sides, rounding out the 360-degree architecture of the building design. Active uses line the public street frontages with multiple retail spaces lining Blake Road and anchoring the southeast building corner. Shared building amenity spaces for residents will front Excelsior Boulevard with the primary residential entrance at the intersection of Excelsior and the new north-south internal road. Building accent lighting will enhance the building architecture throughout the site.

The elevations along Blake Road and Excelsior Boulevard will meet or exceed the city ordinance minimum requirement of 60% transparency for front street-facing façades (between 2 ft and 8 ft).

Where 60% transparency is not provided along the western elevation facing the private internal road, windows are provided into the parking garage and the wall is comprised primarily of brick. The wall acts as a backdrop for the public linear park and will be planted with trees and vines that will provide another layer of visual interest for passersby that varies with the seasons. The western façade will incorporate recessed detailing along with the glass, accounting for a total of 50% of the elevation (30% glazing and 20% recessed brick) The western drive is a private road created by the development team, so it is unclear if a deviation should be required for this frontage, as this isn't a city street.

The north elevation will receive a similar treatment to the West with the addition of wrapping the retail detailing around the corner. Then elevating the visual interest and aesthetic with a balanced mix of glazing, brick accents and illuminated façade. There is no glazing or transparency requirement as it does not face the street or a public right-of-way, but $\pm 20\%$ tinted/opaque glazing is provided in addition to $\pm 22\%$ façade detailing with a recessed brick accent.

Site Access Improvements

The master plan for the new development aligns directly with goals from Station Area Planning documents. A new network of publicly accessible streets divides the site into pedestrian-scaled, walkable quadrants and connect the surrounding context to the SWLRT station. The new north-south street becomes the crucial spine from which stems primary building entries, active uses, and a public linear park. The east-west street further breaks down an otherwise super block, improving circulation and creating a tree-lined street with parallel parking and walk-up townhome units.

As described under the 'Streetscape and Public Realm' section, the project vision includes extensive pedestrian improvements for the area, including sidewalk connections through the site that do not exist today. Both the trees and parallel parking zones will help to create visible friction that will successfully reduce driving speeds to provide a safer environment for people, bikers, and drivers. The pedestrian zone along Excelsior Boulevard (ranges from 28' to 34 ft) and Blake Road (ranges from 17'-6" to 34 ft) expands to a width that supports public street amenities including benches, bike racks, street lighting and planting beds.

The deviations for building setbacks are a result of the unique property configurations at this site, and the proposed setbacks provide comfortable buffer zones between street right-of-way and the building edges. The proposed setbacks offer a balance of enough distance to create green zones without compromising the more urban goals of transit-oriented development. This balance of ample though not excessive setbacks prioritizes pedestrians and human activity in and around the site.

Along the northern edge of the site, a sidewalk is proposed within the rear yard. While this is not along a frontage road and therefore not required, Trilogy believes this added pedestrian pathway through the site will be a great amenity for the adjacent community. Providing this east-west connection will allow those walking along Blake Road or coming from the LRT station to access the interior of the site and the pocket park without having to walk up to Excelsior Blvd to do so. The sidewalk will be well lit with building mounted and bollard lighting. Native plantings will line the majority of the pathway, and decorative screening and a wood fence will provide visual interest along the way.

Exceeding Stormwater Standards

The applicant is exceeding the Minnehaha Creek Watershed District and City of Hopkins' minimum standards for stormwater treatment. Runoff from this Site will be conveyed to the future 325 Blake Road regional stormwater treatment facilities via the Powell Road diversion. However, the new landscape areas, underground pipe retention systems and Jellyfish treatment structures proposed on-site will not only provide rate control but will treat the first 2-inches of rainfall from the impervious area. This is a substantial improvement from the site condition today as the site is primarily impervious surface and stormwater runs off without any treatment or attenuation.

In summary, the new landscaping will help with stormwater abstraction and the underground treatment system will retain and treat stormwater in excess of MCWD and City standards. The proposed on-site stormwater management system exceeds MCWD/City standards by providing filtration treatment above the required 2" rainfall event. The detention systems (BMP 2A and BMP 2C) have the volume capacity to treat up to a 2.55" rainfall event. The proposed Jellyfish Filter systems (BMP 2B and BMP 2D) have the capacity to treat a flow rate of 1.30 CFS and 0.85 CFS, respectively. The corresponding modelled flow rates are 1.24 CFS and 0.72 CFS, which are within the filtration system treatment capacities. MCWD also requires 70% TSS removal, whereas BMPs 2B and 2D have an 80% removal efficiency. With 88% of Site capture, the filtration system provides 71% TSS removal.

MCWD requires proposed runoff rates to be equal to or less than the existing condition. Total proposed site runoff rates are reduced by approximately 26-66% from the existing condition, depending on the rainfall event.

The majority of the new/reconstructed site area will be dedicated to roof drainage, which is assumed cleaner than pavement runoff. This will further reduce pollutant loadings discharged from the site. The landscaped area within the study area is almost doubled from the existing conditions as well. Due to contaminated groundwater conditions, this additional landscape area has not been included in the model for viable infiltration to be conservative, but the new landscape areas will certainly provide additional volume reduction in reality.

Enhanced Landscaping

The deviations in setbacks offer yards that are both deep enough for landscaping and green space and reasonable for creating a "street wall" for the pedestrian experience. The deviations also help to implement a comprehensive plan goal of creating a 'positive relationship to the street' by proposing appropriate setbacks for the residential density guided by the 2040 Comprehensive Plan (75-150 units per acre within ¼ mile of an LRT station platform).

As previously mentioned under 'Streetscape and Public Realm,' the master plan includes generous planted zones (6 ft to 12 ft wide) and a rhythm of deciduous street trees that will provide a human-scale canopy year-round. The trees' summer shade reduces the heat island effect, creating a comfortable microclimate when needed most.

A linear public park is proposed on the western edge of Building B. The pocket park offers opportunities for a quick hangout spot or a chance for larger community gatherings to occur. A central plaza space of crushed stone, column lights, artful concrete 'asterisk' blocks and moveable café tables/chairs provide flexible seating arrangements. A mini stage for small performances is proposed along with some nature play balance sculptural elements. Additional gabion wall seating is provided along the edges. A spot for a food truck offers potential food options. Native landscape plantings and trees contributes to a more natural backdrop.

The streetscape will be greatly improved with new planting beds along Excelsior and Blake Road. Raised curb planting beds will be similar to Phase A and provide a visual continuity along Excelsior. New trees and plantings will help extend the existing Blake Road streetscape. Building edges are enhanced by green and planted yards with an array of site-appropriate species. This variety adds a visually interesting green environment and is a significant improvement from existing conditions today.

Calculating the minimum landscape value based on the current project budget for Site B and per the City's landscape code requirements (section 102-375 Landscaping), the minimum landscape value is approximately \$103,000, with at least 25% allocated to pedestrian improvements. The estimated Landscape and Pedestrian/Streetscapes value for Site B will exceed the minimum landscape value requirement.

We estimate that Site B will provide the following:

- Trees: \$33,000
- Shrubs: \$25,000
- Grasses: \$12,000

- Sod: \$6,000
- Planting Soil: \$25,000
- Edger/Mulch: \$22,000
- Irrigation: \$40,000
- Fencing/screening: \$25,000

Excelsior & Blake Boulevards

- Curb islands: \$32,000
- Pavers: \$26,000
- Site Furniture: \$15,000
- Lighting: \$25,000

TOTAL ESTIMATED LANDSCAPE VALUE PROVIDED \$286,000

Specifics for Site B’s landscaping plan and plant species can be found within the application materials.

Proposed Pet Exercise Area (Dog Park) & Relief Areas

The importance of providing a formal exercise area for pets, specifically dogs, has been acknowledged by including a dog park along the west side of the future Site C building. The dog park will be fenced and includes small-scale lighting, seating and receptacles for proper disposal of animal waste.

A dog relief area will be provided for use by residents along the west side of Building B.

Bike Amenities

With immediate access to the Cedar Lake Trail, ample bike amenities will promote the use of multiple modes of transportation for residents and patrons to the site. The city standard for bike parking is one long term stall per 2 units and one short term per 20 units (138 total bike racks). Building B will accommodate over 150 bike racks for resident storage within the building. The Mixed Use district bicycle parking requirements are 0.5 long-term spaces per employee and 0.5 spaces per 1,000 square feet of net building area or 5 spaces. Public bike racks (20 capacity) are located within the right-of-way adjacent to the primary Building B entrance as well as adjacent to the retail uses along Blake Road. A bike wash area with automatic shut off timer will be available in the P1 parking level. The development team is open to providing space on site to accommodate city micro mobility solutions in the future.

Sustainable Design

The project is enrolled in the Xcel Energy Design Assist (EDA) program to evaluate and prioritize strategies in order to maximize energy efficiency. See separate attachment for a comprehensive summary of the energy saving and sustainable design features of the project.

Support of Comprehensive Plan Goals

The project supports and further advances a number of goals outlined in the City’s 2030 Comprehensive Plan and draft 2040 Cultivate Hopkins Plan. See summary of these goals below within the PUD statement section.

PLANNED UNIT DEVELOPMENT STATEMENT

Per Hopkins, Minnesota, Code of Ordinances, Part III – Land Development, Chapter 102 – Planning and Zoning, Article XX. – Planned Unit Development, Sec. 102-680, (e), (6), the applicant is responding to the following:

- A written statement generally describing the proposed PUD and the market which it is intended to serve and its demand showing its relationship to the city’s comprehensive plan and how the proposed PUD is to be designed, arranged and operated in order to permit the development and use of neighboring property in accordance with the applicable regulations of the city.

Market

The proposed Planned Unit Development is intended to serve the growing population of Hopkins and provide dwelling units within one quarter of a mile of the future Blake Road Southwest LRT Station. It is anticipated with the proposed future LRT station that this site will be a regional draw for apartment-dwellers.

Trilogy will deliver an incredibly important development node along Blake Road, as envisioned in numerous planning documents, including the East Hopkins Land Use & Market Study, Blake Road Corridor Study and the Blake Road LRT Station Area Development Guidelines. This project will be an anchor for the LRT station and act as a catalyst for future development in the area. Many of the development recommendations outlined in the station area development guidelines are being realized within the proposed project. Here are just a few of the stated goals that will be supported by the project’s site and building design:

Site Design:

- Create a connected, walkable, mixed-use, sustainable neighborhood, with a pedestrian-oriented and human-scale streetscape
- Improve safety and security with strategies such as greater security presence, improved lighting, and other practices such as CPTED (Community Policing Through Environmental Design)
- Pursue transit-oriented design that enhances multi-modal access, and provide for bicycle accommodations (racks, lockers, etc.) for businesses and residents.
- Add green space to soften the built environment that would allow for outdoor use year-round.
- Utilize landscaping and streetscape amenities to create stronger pedestrian district.
- Limit surface parking with new development encourage underground or structured parking

Neighborhood Diversity:

- Provide for a range of housing types and price points to meet the needs of people in all stages of life, with the design flexibility to accommodate changing lifestyle needs
- Utilize universal design principles that can respond to changing demographic needs and anticipate in innovative ways to address the dynamic and changing needs of residents
- Strengthen the vitality of the area through increased density and mixed commercial and residential uses
- Promote high-quality design
- Enhance a sustainable neighborhood by promoting energy efficiency and renewable energy
- Create opportunities to live, work, learn, play – the spectrum of elements for a healthy community

Sustainable Design:

- Improve water and environmental quality
- Incorporate sustainable development practices into new construction projects

Neighborhood vitality and livability:

- Multi-family housing amenities such as guest suites, work-from-home opportunities, and shared space for larger gatherings
- Medium to high density residential, to preserve green space and enhance street-level amenities
- Locally owned businesses and increased opportunities for residents of all ages to live and work in the area
- The potential for commercial uses that enhance rather than compete with downtown Hopkins Mainstreet vitality
- Creative ways to support small cultural businesses that serve the community
- Convenience services (e.g. pet maintenance areas, bike repair, etc.), especially near the transit station
- Flexible space that can adapt as needs change
- Welcome developers and businesses that operate with equity principles of hiring and wages

Comprehensive Plan

The 2040 Comprehensive Plan Update – Cultivate Hopkins – guides this property as Activity Center. Activity Centers surround and support the planned Blake Road and Shady Oak light rail stations along the Southwest LRT Green Line Extension. These areas will include moderate density to high-density mixed-use development designed to complement and enhance the existing development pattern in these areas and support the public investment in transit. The Activity Center areas are expected to experience significant reinvestment and redevelopment to absorb a substantial portion of the city’s anticipated future growth.

Development in the Activity Center areas is expected to be medium to larger scale neighborhood and regional uses with an approximate mix of 75% residential and 25% commercial. Densities in these areas will typically range from 20-60 units per acre, with 75-150 units per acre within ¼ mile of an LRT station platform.

The proposed project illustrates a residential density that is consistent with the Activity Center guidance, but it falls short of the 2040 guidance for commercial space percentage. Site A will provide 2,000 sq ft of retail space along Excelsior Boulevard and Site B will provide 10,000 sq ft of retail space fronting Blake Road. Trilogy is responding to current market conditions that are more immediate than a comprehensive plan’s land use guidance. The Retail Component Evaluation performed concluded that retail in this development is high-risk. The applicant is attempting to work with the City of Hopkins to address the retail desire, but also needs to be vigilant and not over supply the site. There are negative ramifications to continually vacant retail space. The applicant believes strongly that multi-family residential is highest and best use for this location. The constraints of the site are prohibitive to adding traditional retail space to the development.

The Applicant is responding to the 2040 comprehensive plan with an overall development project that supports the following stated goals:

- Encourage transit-oriented development (development that emphasizes pedestrian and bicycle connectivity and a broader mix of uses at densities that support transit) in areas with high quality transit service, especially within a quarter mile of light rail stations or high-frequency bus

routes.

- Plan for appropriate amenities, high-quality design, pedestrian and bicycle facilities, and open space in high growth areas, particularly in the Neighborhood Center, Activity Center, and Downtown Center future land use categories or other areas in close proximity to transit.
- Encourage the transition of selected auto-oriented areas into Activity Centers, as defined and designated in the comprehensive plan.
- Improve pedestrian and bicycle access throughout the community, particularly in the Centers future land use categories as defined and designated in the comprehensive plan or other areas in close proximity to transit.
- Engage the community to explore how to increase the mix of housing types near transit corridors, parks, and the Centers future land use categories as defined and designated in the comprehensive plan.
- Encourage all new projects to have a positive relationship to the street by orienting main entrances to the front of the property, connecting the front door to the sidewalk, and reducing parking between the building and the street as much as possible.

Designed, Arranged, Operated

This project will not impede improvement of surrounding properties, and the PUD is designed and arranged to permit the orderly development and use of neighboring property in accordance with the applicable regulations of the City. The organization of the parcels within this development is specifically intended to create a walkable, transit-oriented community. The height and associated density of the project will provide housing to help meet City goals for increasing housing options in Hopkins along transit corridors. The proposed residential and retail uses are compatible with and will enhance the existing character of the area. High-density, mixed-use redevelopment of this site is consistent with City goals and, by being responsive to the land use policies for this area, the project will promote the orderly development of the Blake Road Corridor.

The contemporary, high-quality building design will be an attractive addition to this important intersection at Blake Road and Excelsior Boulevard, and the mix of uses will improve and activate the pedestrian realm. The proposed development will not be detrimental to or endanger the public health, safety, comfort or general welfare, and approving the PUD will allow a transit-oriented, mixed-use development that is consistent with the goals of City's comprehensive plan. Infill development on the underutilized site will have a positive effect on the health, safety and vitality of the area. The new construction will comply with all building and site development codes.

Project: Blake Road Station Development
RE: Neighborhood Meeting (March 8, 2022, 6:00-7:30 pm)

Attendees: (see attached Sign-In Sheet for attendees that signed in; approximately 13 people attended)

Development Team Attendees:

Josh Peters, Tim Biere, Nicole Midtdahl, Chad Olmschenk – Trilogy Real Estate Group
Gretchen Camp, Jeff Hemer, Brent Holdman, Logan Brandes – ESG
Jesse Symynkywicz – Damon Farber

Overall themes/topics from attendees:

- Affordable housing
- Inquiries from current and prospective tenants about retail space
- Project timeline
- Pedestrian connections and improvements

Questions/comments from discussions:

1. Is there affordable housing within the development? If getting public subsidy, then should really provide affordable housing on the site. **Currently, no affordable units are proposed for Site A, B or C. Trilogy will continue to review feasibility for adding affordable units to Site C, but this will require assistance from City/State sources.**
2. Will there be minority or women-owned sub-contractors constructing the project? **There is currently no requirement for MBE or WBE sub-contractors. Trilogy will inquire with the general contractor, Kraus-Anderson, on tracking MBE/WBE participation on the project.**
3. Concerns about height; too many units on the site. Worried about neighborhood turning into a multi-family housing area.
4. Previous rent was much lower than what will be charged for the new spaces.
5. What is the project timeline? **Site A has started construction. Site B will break ground in late summer/early fall 2022. Site C is several years out due to existing leases.**
6. Add Somali people to the project renderings – make them feel welcome.
7. The Alliance can be a resource – equity scorecard for the project.
8. How wide are sidewalks through the site? **See landscape sections for details.** Excited about the project and want to make sure internal roads are safe and people aren't speeding through the site. Can the crosswalks be raised? Looking forward to seeing these improvements for pedestrians.

9. When did the mailings get sent out? **February 22**
10. Work to incorporate affordability for Site C. Commit to engaging community for Site C.
11. Request to provide a 5,000 sq ft retail space (make one part of retail larger).
12. Blake Grocery tenants – interested in moving back into one of the retail spaces.
13. Retail tenants – provide right of first refusal to existing tenants.

Blake Road Station Mixed-Use Development

March 8, 2022 - Neighborhood Meeting Sign-In Sheet

Attendees

	Name	Address	Contact Info (email/phone)
1	Robert Sandy Murray	1409 Boyce St	952-933-6451
2	Butz		
3	Butz		
4	Herrri Fischer	TCA Food Shelf	herrri.fischer@gmail.com
5	Lee Gilbertson	Mac's Liquor	Mac's Liquor@gmail.com 612-481-3678
6	Eric Anderson	53 Jackson Ave. S	keoth@icloud.com
7	Larry Hiscock	302 7th St. S. Hopkins, MN	larryhiscock@gmail.com
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15			

Blake Station Site B

BLAKE STATION
SITE B
Hopkins, MN

8490 Excelsior Boulevard Hopkins, MN

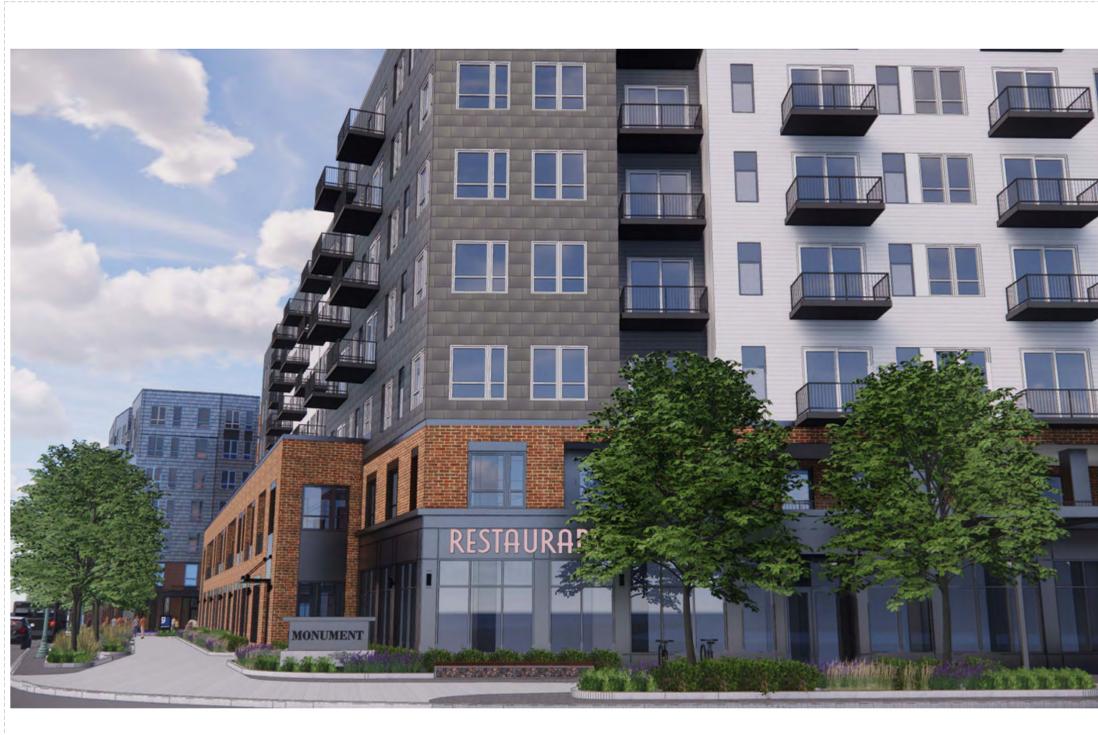


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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota

Signature _____
Typed or Printed Name _____
License # Date _____

NOT FOR CONSTRUCTION



UNIT MIX & BUILDING DATA

**UNIT MIX SUMMARY - OPTION 1)				
Name	Qty	%	Area	Min - Max SF
1 BED	97	39%	66,839 SF	645 SF ... 720 SF
1 BED + DEN	2	1%	1,572 SF	774 SF ... 797 SF
2 BED	80	32%	81,097 SF	904 SF ... 1,052 SF
3 BED	11	4%	14,087 SF	1,272 SF ... 1,318 SF
ALCOVE	40	16%	21,656 SF	495 SF ... 597 SF
STUDIO	20	8%	9,650 SF	464 SF ... 521 SF
Grand total:	250		194,900 SF	

BUILDING GROSS BUILDING	
Level	Area
LEVEL P1	66,337 SF
LEVEL 1	64,904 SF
LEVEL 2	65,004 SF
LEVEL 3	42,027 SF
LEVEL 4	41,828 SF
LEVEL 5	41,828 SF
LEVEL 6	41,828 SF
LEVEL 7	40,812 SF
	404,569 SF

PARKING SCHEDULE - VEHICLES	
Description	Count
LEVEL P1	
8 x 18 COMPACT	7
9'-0" x 16' COMPACT	3
ACCESSIBLE STALL	4
COMPACT	8
STANDARD	126
LEVEL 1	
8 x 18 COMPACT	3
8'-6" x 18' STANDARD	3
9'-0" x 16' COMPACT	3
9'-0" x 18' STANDARD	1
ACCESSIBLE STALL	4
COMPACT	1
STANDARD	71
LEVEL 2	
8 x 18 COMPACT	6
9'-0" x 16' COMPACT	2
ACCESSIBLE STALL	4
COMPACT	5
STANDARD	88
TOTAL STALLS:	339

PARKING GROSS AREA	
Level	Area
LEVEL P1	62,469 SF
LEVEL 1	39,478 SF
LEVEL 2	42,751 SF
	144,697 SF

DRAWING INDEX

DRAWING NUMBER	DRAWING NAME	LUA 2/18/2022
GENERAL INFORMATION		
T1-1	TITLE SHEET	•
T1-2	CONTEXT MAP & IMAGES	•
T1-3	MASTER PLAN	•
CIVIL		
C200	DEMOLITION PLAN	•
C300	EROSION AND SEDIMENT CONTROL PLAN - PHASE 1	•
C301	EROSION AND SEDIMENT CONTROL PLAN - PHASE 2	•
C401	SITE PLAN - SITE B	•
C501	GRADING PLAN - SITE B	•
C503	STORM SEWER PLAN - SITE B	•
C601	UTILITY PLAN - SITE B	•
V100	EXISTING CONDITIONS SURVEY	•
LANDSCAPE		
L010	GENERAL NOTES	•
L011	REFERENCE NOTES & PLANTING SCHEDULES	•
L100	OVERALL SITE PLAN	•
L111	SITE PLAN BUILDING B	•
L140	LANDSCAPE PLAN - BUILDING B	•
L400	LANDSCAPE SECTIONS	•
L500	LANDSCAPE DETAILS	•
ARCHITECTURAL		
A1-0	LEVEL P1 PLAN	•
A1-1	LEVEL 1 PLAN	•
A1-2	LEVEL 2 PLAN	•
A1-3	LEVEL 3 PLAN	•
A1-4	LEVEL 4-6 PLAN	•
A1-5	LEVEL 7 PLAN	•
A1-6	ROOF PLAN	•
A3-1	PERSPECTIVE VIEWS	•
A3-2	SOUTH AND EAST BUILDING ELEVATIONS	•
A3-3	WEST AND NORTH BUILDING ELEVATIONS	•
A3-4	EXTERIOR MATERIALS	•
A3-5	SHADOW STUDIES	•
ELECTRICAL		
E0.1P	ELECTRICAL SITE PHOTOMETRIC	•
E0.2P	ELECTRIC SITE PHOTOMETRIC DETAILS	•
E0.3P	ELECTRIC SITE PHOTOMETRIC DETAILS	•
E0.4P	ELECTRIC SITE PHOTOMETRIC DETAILS	•

PROJECT LOCATION



Vicinity



Site Location

PROJECT TEAM

OWNER/DEVELOPER: Trilogy Real Estate Group, LLC
520 West Erie Street Suite 100
Chicago, IL 60654
Ph: (312) 750-0900 Ext. 125

ARCHITECT: Elness Swenson Graham Architects, Inc.
500 Washington Ave. South, Suite 1080
Minneapolis, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382

CIVIL ENGINEER: Kimley-Horn
767 N Eustis Street, Suite 100
St. Paul, MN 55114
Ph: 651-645-4197

LANDSCAPE ARCHITECT: Damon Farber Landscape Architects
310 S 4th Ave, Suite 7050
Minneapolis, MN 55415
Ph: 612-332-7522

STRUCTURAL ENGINEER: Meyer Borgman Johnson
510 S Marquette Ave Unit 900
Minneapolis, MN 55402
Ph: 612-338-0713

MEP ENGINEERING: Emanuelson-Podas, Inc.
7705 Bush Lake Road
Edina, MN 55439
Ph: 952-930-0050

LUA Submittal
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02/18/22

REVISIONS:
No. Description Date

221561.00
PROJECT NUMBER

Author _____ Checker _____
DRAWN BY _____ CHECKED BY _____

KEY PLAN

BLAKE STATION SITE B

TITLE SHEET

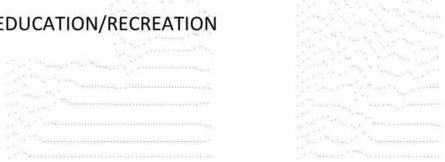
T1-1

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Signature _____
Typed or Printed Name _____
License # Date _____



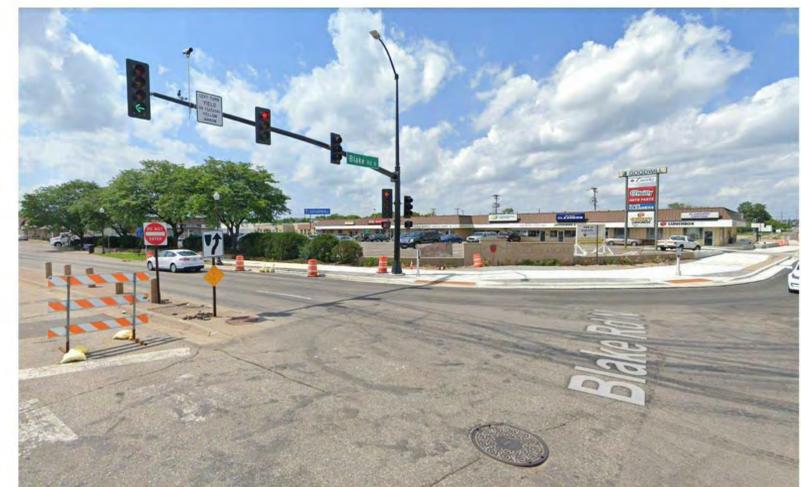
- MULTIFAMILY RESIDENTIAL
- COMMERCIAL
- INDUSTRIAL/WAREHOUSE
- EDUCATION/RECREATION



1. FROM WEST OF EXCELSIOR BLVD, LOOKING NORTHEAST



2. EXCELSIOR BLVD, LOOKING NORTHWEST



3. FROM EXCELSIOR BLVD & BLAKE RD, LOOKING NORTHWEST

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CONSTRUCTION

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KEY PLAN

BLAKE STATION SITE B

CONTEXT MAP & IMAGES

T1-2

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MASTER PLAN



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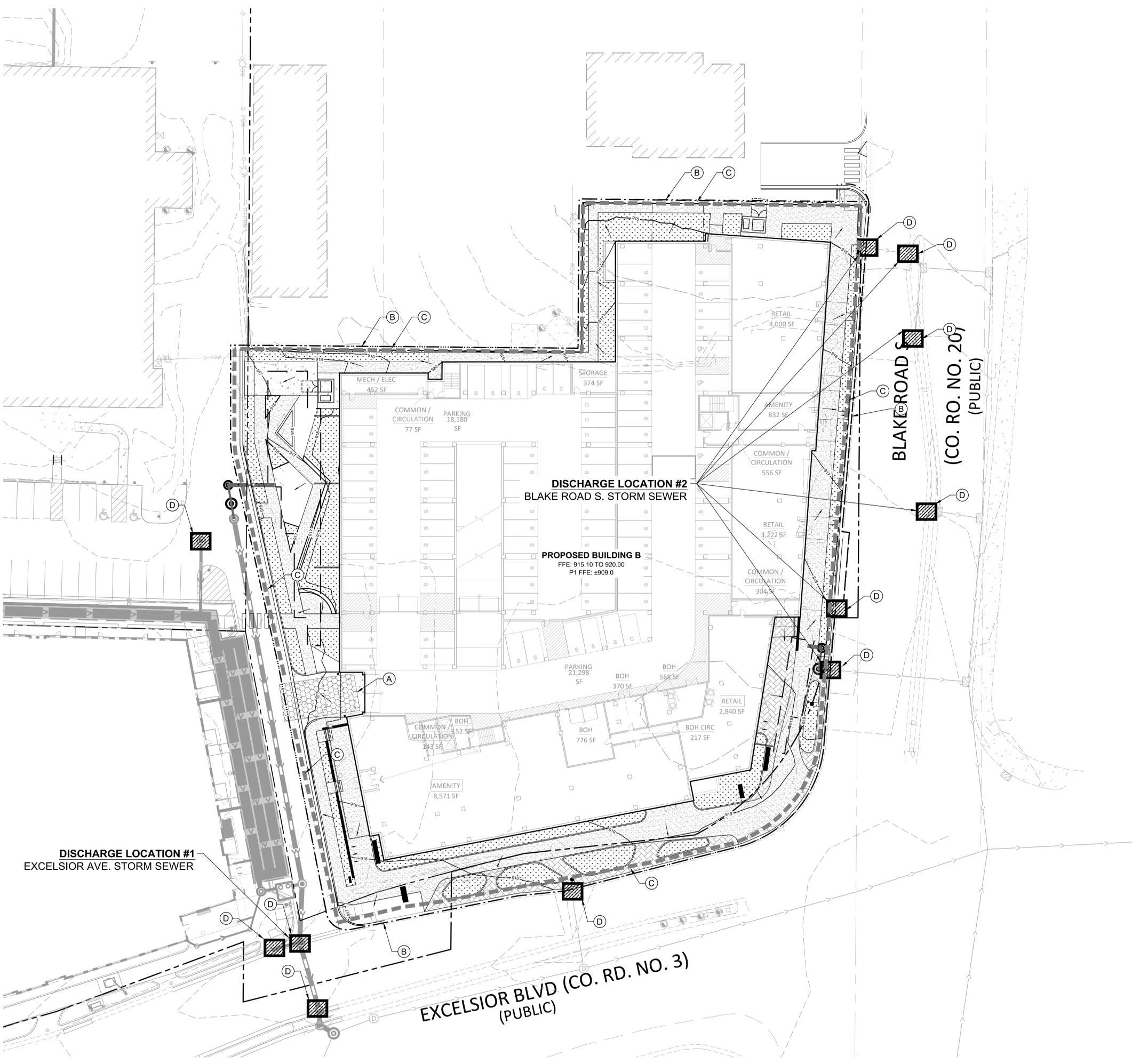
221561.00
PROJECT NUMBER

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KEY PLAN

BLAKE STATION SITE B

MASTER PLAN
T1-3



- LEGEND**
- ROCK ENTRANCE
 - EROSION CONTROL BLANKET
 - INLET PROTECTION
 - SILT FENCE
 - LIMITS OF DISTURBANCE
 - SAFETY FENCE
 - BIROLL

- EROSION CONTROL PLAN NOTES**
1. ALL PERIMETER SILT FENCE AND ROCK CONSTRUCTION ENTRANCES SHALL BE INSTALLED PRIOR TO CONSTRUCTION.
 2. THE CONTRACTOR SHALL CONSTRUCT DRAINAGE BASINS PRIOR TO SITE GRADING.
 3. THE CONTRACTOR SHALL INSTALL CATCH BASIN EROSION CONTROL MEASURES.
 4. WITHIN TWO WEEKS (14 DAYS) OF SITE GRADING, ALL DISTURBED AREAS SHALL BE STABILIZED WITH SEED, SO2, OR ROCK BASE. REFER TO LANDSCAPE PLANS FOR MATERIALS.
 5. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED AND MAINTAINED IN ACCORDANCE WITH CITY, STATE, AND WATERSHED DISTRICT PERMITS.
 6. THE CONTRACTOR SHALL MAINTAIN ALL EROSION CONTROL MEASURES, INCLUDING THE REMOVAL OF SILT IN FRONT OF SILT FENCES DURING THE DURATION OF THE CONSTRUCTION.
 7. ANY EXCESS SEDIMENT IN PROPOSED BASINS SHALL BE REMOVED BY THE CONTRACTOR.
 8. REMOVAL ALL EROSION CONTROL MEASURES AFTER VEGETATION IS ESTABLISHED.
 9. THE CONTRACTOR SHALL REMOVE ALL SOILS AND SEDIMENT TRACKED ONTO EXISTING STREETS AND PAVED AREAS AND SHALL SWEEP ADJACENT STREETS AS NECESSARY IN ACCORDANCE WITH CITY REQUIREMENTS.
 10. IF BLOWING DUST BECOMES A NUISANCE, THE CONTRACTOR SHALL APPLY WATER FROM A TANK TRUCK TO ALL CONSTRUCTION AREAS.

- SEQUENCE OF CONSTRUCTION:**
- UPON IMPLEMENTATION AND INSTALLATION OF THE FOLLOWING AREAS: TRAILER, PARKING, LAYDOWN, PORTA-POTTY, WHEEL WASH, CONCRETE WASHOUT, FUEL AND MATERIAL STORAGE CONTAINERS, SOLID WASTE CONTAINERS, ETC. IMMEDIATELY DENOTE THEM ON THE SITE MAPS AND NOTE ANY CHANGES IN LOCATION AS THEY OCCUR THROUGHOUT THE CONSTRUCTION PROCESS.
- BMP AND EROSION CONTROL INSTALLATION SEQUENCE SHALL BE AS FOLLOWS:
1. TEMPORARILY SEED THROUGHOUT CONSTRUCTION, DENuded AREAS THAT WILL BE INACTIVE FOR 7 DAYS OR MORE.
 2. INSTALL UTILITIES, UNDERDRAINS, STORM SEWERS, UNDERGROUND SYSTEM, CURBS AND GUTTERS.
 3. INSTALL APPROPRIATE INLET PROTECTION AT ALL STORM SEWER STRUCTURES AS EACH INLET STRUCTURE IS INSTALLED.
 4. PERMANENTLY STABILIZE AREAS TO BE VEGETATED AS THEY ARE BROUGHT TO FINAL GRADE.
 5. PREPARE SITE FOR PAVING.
 6. PAVE SITE AND INSTALL STRIPING.
 7. INSTALL APPROPRIATE INLET PROTECTION DEVICES FOR PAVED AREAS AS WORK PROGRESSES.
 8. COMPLETE GRADING AND INSTALLATION OF PERMANENT STABILIZATION OVER ALL AREAS.
 9. OBTAIN CONCURRENCE WITH THE CIVIL ENGINEERING CONSULTANT THAT THE SITE HAS BEEN FULLY STABILIZED THEN:
 1. REMOVE ALL REMAINING TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES
 2. STABILIZE ANY AREAS DISTURBED BY THE REMOVAL OF BMPs.

- KEYNOTE LEGEND**
- (A) CONSTRUCTION ENTRANCE
 - (B) LIMITS OF DISTURBANCE, OFFSET FOR CLARITY
 - (C) BIROLL
 - (D) INLET PROTECTION
 - (E) BIOLOG

BMP QUANTITIES		
BMP	UNIT	QUANTITY
CONSTRUCTION ENTRANCE	EA.	1
BIROLL	LF	1,275
INLET PROTECTION	EA.	12

**BLAKE STATION
SITE B**

Hopkins, MN

Kimley»Horn
302 KIMLEY HORN AND ASSOCIATES, INC.
767 EAST 18TH STREET, SUITE 100, ST. PAUL, MN 55114
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Signature
WILLIAM D. MATZEK
Typed or Printed Name
42661 XX/XX/XXXX
License # Date

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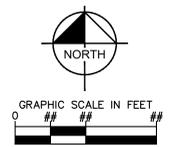
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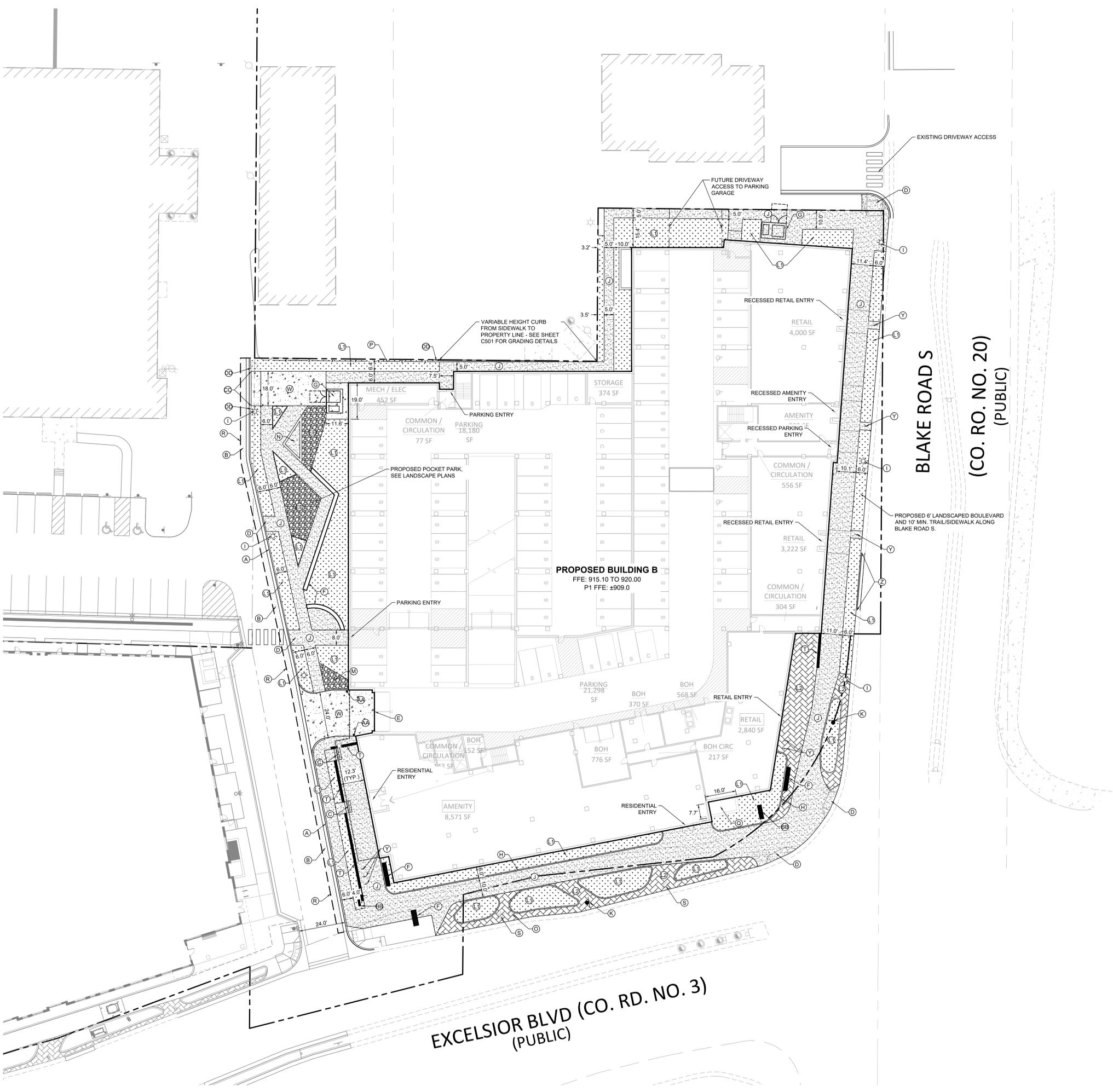
KEY PLAN

BLAKE ROAD STATION -
SITE B

EROSION AND SEDIMENT
CONTROL PLAN - PHASE 2

C301





LEGEND

[Symbol]	PROPERTY LINE
[Symbol]	RETAINING WALL
[Symbol]	PROPOSED CURB AND GUTTER
[Symbol]	PROPOSED LANDSCAPING, SEE LANDSCAPE PLAN
[Symbol]	PROPOSED CRUSHED STONE LANDSCAPING, SEE LANDSCAPE PLAN
[Symbol]	PROPOSED HARDSCAPE, SEE LANDSCAPE PLANS
[Symbol]	PROPOSED STANDARD DUTY ASPHALT
[Symbol]	PROPOSED CONCRETE PAVEMENT
[Symbol]	PROPOSED CONCRETE SIDEWALK

PROPERTY SUMMARY

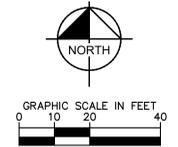
BLAKE & EXCELSIOR - SITE B	
TOTAL PROPERTY AREA	7.39 AC
RIGHT OF WAY DEDICATION	0.00 AC
BUILDING A PARCEL	1.62 AC
BUILDING B PARCEL	2.27 AC
BUILDING C PARCEL (FUTURE)	3.50 AC
PROPOSED IMPERVIOUS AREA (BUILDING B PARCEL)	85,393 SF (1.96 AC)
PROPOSED PERVIOUS AREA (BUILDING B PARCEL)	8,939 SF (0.21 AC)
TOTAL DISTURBED AREA (INCLUDES OFF-SITE IMPROVEMENTS)	98,331 SF (2.26 AC)

ZONING SUMMARY

EXISTING ZONING	B-3 GENERAL BUSINESS
PROPOSED ZONING	PUD

- SITE PLAN NOTES**
- ALL WORK AND MATERIALS SHALL COMPLY WITH ALL CITY/COUNTY REGULATIONS AND CODES AND O.S.H.A. STANDARDS.
 - CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATIONS AND DIMENSIONS OF VESTIBULES, SLOPE PAVING, SIDEWALKS, EXIT PORCHES, TRUCK DOCKS, PRECISE BUILDING DIMENSIONS AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
 - ALL DIMENSIONS AND RADII ARE TO THE FACE OF CURB UNLESS OTHERWISE NOTED.
 - EXISTING STRUCTURES WITHIN CONSTRUCTION LIMITS ARE TO BE ABANDONED, REMOVED OR RELOCATED AS NECESSARY. ALL COST SHALL BE INCLUDED IN BASE BID.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ALL RELOCATIONS, (UNLESS OTHERWISE NOTED ON PLANS) INCLUDING BUT NOT LIMITED TO, ALL UTILITIES, STORM DRAINAGE, SIGNS, TRAFFIC SIGNALS & POLES, ETC. AS REQUIRED. ALL WORK SHALL BE IN ACCORDANCE WITH GOVERNING AUTHORITIES REQUIREMENTS AND PROJECT SITE WORK SPECIFICATIONS AND SHALL BE APPROVED BY SUCH. ALL COST SHALL BE INCLUDED IN BASE BID.
 - SITE BOUNDARY, TOPOGRAPHY, UTILITY AND ROAD INFORMATION TAKEN FROM A SURVEY BY SAMBATEK, DATED 02/12/2020. KIMLEY-HORN ASSUMES NO LIABILITY FOR ANY ERRORS, INACCURACIES, OR OMISSIONS CONTAINED THEREIN.
 - CONTRACTOR SHALL REFERENCE ARCH / MEP PLANS FOR SITE LIGHTING AND ELECTRICAL PLAN.
 - NO PROPOSED LANDSCAPING SUCH AS TREES OR SHRUBS, ABOVE AND UNDERGROUND STRUCTURES, OR OTHER OBSTRUCTIONS SHALL BE LOCATED WITHIN EXISTING OR PROPOSED UTILITY EASEMENTS AND RIGHTS OF WAY UNLESS SPECIFICALLY NOTED ON PLANS OTHERWISE.
 - REFER TO FINAL PLAT OR ALTA SURVEY FOR EXACT LOT AND PROPERTY BOUNDARY DIMENSIONS.
 - ALL AREAS ARE ROUNDED TO THE NEAREST SQUARE FOOT.
 - ALL DIMENSIONS ARE ROUNDED TO THE NEAREST TENTH FOOT.

- KEYNOTE LEGEND**
- (A) B612 CURB AND GUTTER
 - (B) STANDARD DUTY ASPHALT PAVEMENT
 - (C) PROPOSED STOOP/STAIRS/RISERS, REFERENCE ARCHITECTURAL PLANS FOR DETAILS
 - (D) ACCESSIBLE CURB RAMP
 - (E) PROPOSED GARAGE DOOR, REFERENCE ARCHITECTURAL PLANS FOR DETAILS
 - (F) PROPOSED BENCH/SITE FURNITURE, REFERENCE GRADING PLAN FOR ELEVATIONS AND LANDSCAPE PLANS FOR DETAILS
 - (G) PROPOSED TRANSFORMER PAD, UTILITY SCREEN/ENCLOSURES, REFERENCE ARCHITECTURAL PLANS FOR DETAILS
 - (H) PROPOSED PLANTER BED, REFERENCE LANDSCAPE ARCHITECTURAL PLANS FOR DETAILS
 - (I) PROPOSED STREET LIGHT, REFERENCE LIGHTING PLANS FOR DETAILS
 - (J) PROPOSED SIDEWALK
 - (K) RELOCATED TRAFFIC SIGN, SEE DEMO PLAN FOR EXISTING LOCATIONS
 - (L) PROPOSED LANDSCAPE AREA, REFERENCE LANDSCAPE ARCHITECTURAL PLANS FOR DETAILS
 - (M) PROPOSED HARDSCAPE AREA, REFERENCE LANDSCAPE ARCHITECTURAL PLANS FOR DETAILS
 - (N) PET RELIEF AREA, SEE ARCH PLANS
 - (O) RAISED STAGE FEATURE, SEE LANDSCAPE PLANS
 - (P) EXISTING TRAFFIC SIGN TO REMAIN
 - (Q) PROPOSED FENCE
 - (R) EXHAUST WELL, SEE ARCH PLANS
 - (S) SAWCUT LINE/MATCH EXISTING PAVEMENT
 - (T) SALVAGED AND RELOCATED STREET LIGHT - CONTRACTOR TO SALVAGE AND PROVIDE LIGHT POLES TO CITY FOR STORAGE. CONSTRUCT LIGHT POLE BASES AND PROVIDE CONDUIT WIRING, REINSTALL LIGHT POLES
 - (U) PROPOSED RETAINING WALL, REFERENCE GRADING PLAN FOR ELEVATION DETAILS AND LANDSCAPE PLANS FOR WALL TYPE
 - (V) FIRE LANE SIGN, SEE DETAIL
 - (W) NOT USED
 - (X) CONCRETE PAVEMENT DRIVEWAY ENTRANCE
 - (Y) SALVAGE AND RELOCATE EXISTING BENCH
 - (Z) PROPOSED BIKE RACKS, REFERENCE ARCHITECTURAL PLANS FOR DETAILS
 - (AA) B624 CURB AND GUTTER
 - (AB) PROPOSED BOLLARD
 - (AC) PROPOSED MONUMENT SIGNAGE, REFERENCE ARCHITECTURAL PLANS
 - (AD) SURMOUNTABLE B612 CURB
 - (AE) TRANSITION CURB
 - (AF) CRUSHED STONE LANDSCAPED AREA, REFERENCE LANDSCAPE PLANS FOR DETAILS



BLAKE STATION SITE B

Hopkins, MN

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 302 KIMLEY-HORN AND ASSOCIATES, INC.
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Signature
 WILLIAM D. MATZEK
 Typed or Printed Name
 42661 XX/XX/XXXX
 License # Date

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160573033
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KEY PLAN

BLAKE ROAD STATION - SITE B

SITE PLAN - SITE B
C401

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KEY PLAN

BLAKE ROAD STATION -
SITE B

GRADING PLAN - SITE B

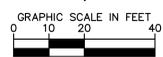
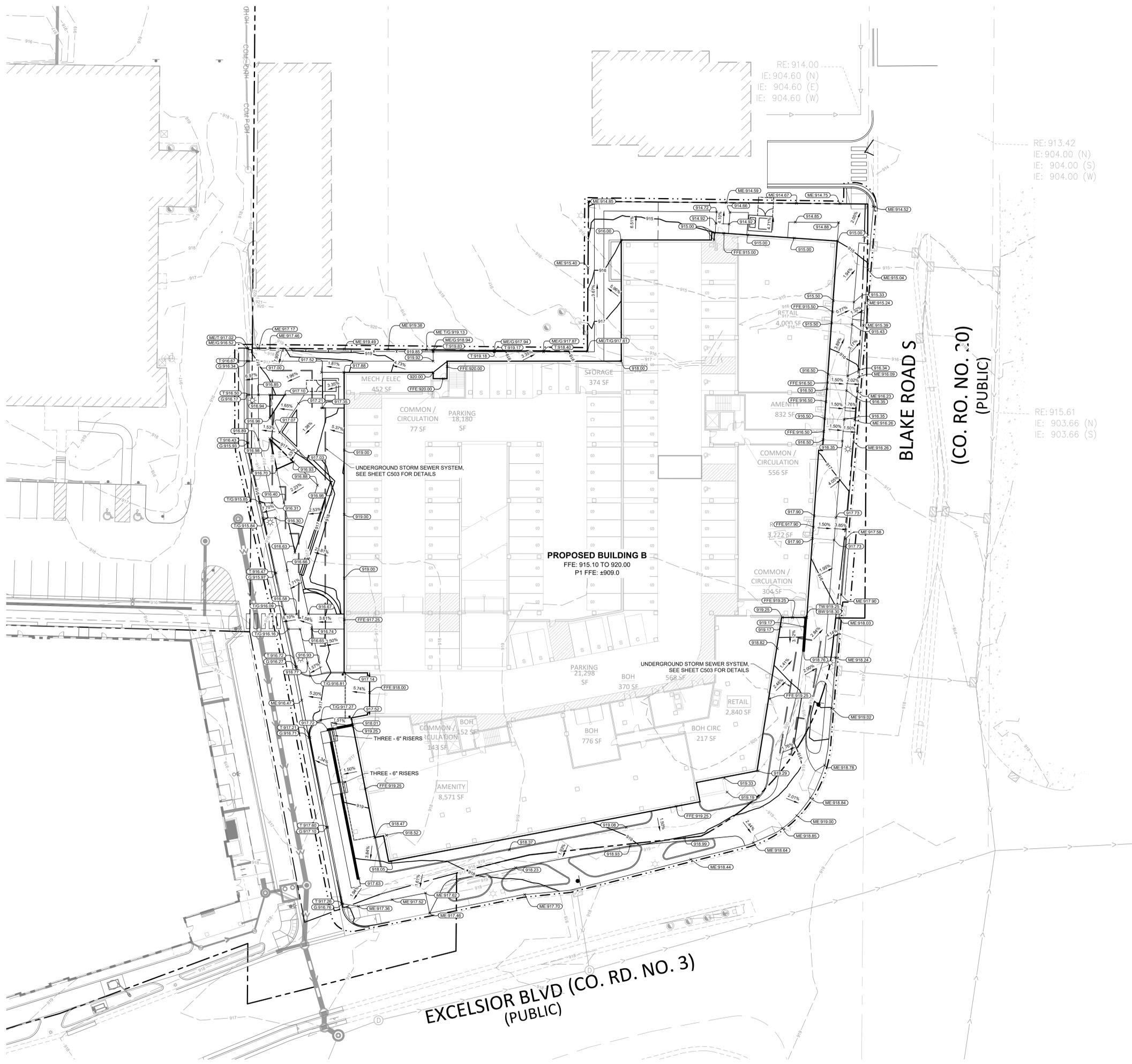
C501

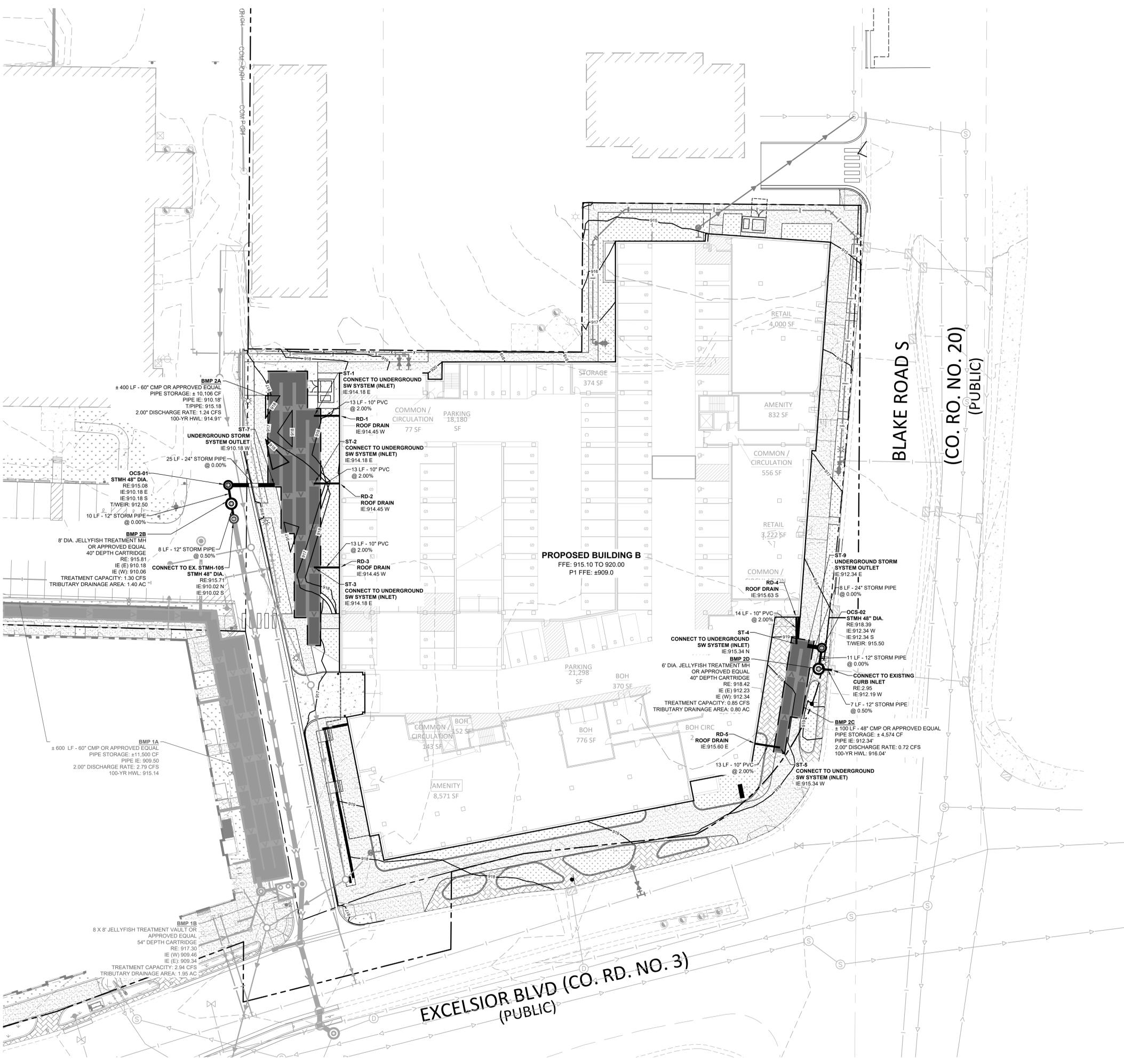
LEGEND

- PROPERTY LINE
- - - EXISTING CONTOUR
- - - PROPOSED CONTOUR
- - - PROPOSED LIMITS OF DISTURBANCE
- PROPOSED STORM MANHOLE (SOLID CASTING)
- PROPOSED STORM MANHOLE (ROUND INLET CASTING)
- PROPOSED STORM MANHOLE/CATCH BASIN (CURB INLET CASTING)
- PROPOSED STORM SEWER CLEANOUT
- PROPOSED FLARED END SECTION
- PROPOSED RIPRAP
- PROPOSED STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED SPOT ELEVATION
- PROPOSED HIGH POINT ELEVATION
- PROPOSED LOW POINT ELEVATION
- PROPOSED GUTTER ELEVATION
- PROPOSED TOP OF CURB ELEVATION
- PROPOSED FLUSH PAVEMENT ELEVATION
- MATCH EXISTING ELEVATION
- PROPOSED EMERGENCY OVERTFLOW
- PROPOSED DRAINAGE DIRECTION
- 0.0% PROPOSED ADA SLOPE

GRADING PLAN NOTES

1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF HOPKINS, SPECIFICATIONS AND BUILDING PERMIT REQUIREMENTS.
2. CONTRACTOR TO CALL GOPHER STATE CALL ONE @ +1-800-252-1166+ AT LEAST TWO WORKING DAYS PRIOR TO EXCAVATION/CONSTRUCTION FOR UTILITY LOCATIONS.
3. STORM SEWER PIPE SHALL BE AS FOLLOWS:
RCP PER ASTM C-76
HOPE: 6" - 10" PER AASHTO M-252
HOPE: 12" OR GREATER PER ASTM F-2386
PVC SCH. 40 PER ASTM D-3034
STORM SEWER FITTINGS SHALL BE AS FOLLOWS:
RCP PER ASTM C-76, JOINTS PER ASTM C-361, C-900, AND C-443
HOPE PER ASTM D-312
PVC PER ASTM D-3034, JOINTS PER ASTM D-3212
4. CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OR EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE START OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER OF ANY DISCREPANCIES OR VARIATIONS.
5. SUBGRADE EXCAVATION SHALL BE BACKFILLED IMMEDIATELY AFTER EXCAVATION TO HELP OFFSET ANY STABILITY PROBLEMS DUE TO WATER SEEPAGE OR STEEP SLOPES. WHEN PLACING NEW SURFACE MATERIAL ADJACENT TO EXISTING PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING OF EXISTING PAVEMENT.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HORIZONTAL AND VERTICAL CONTROL.
7. CONTRACTOR SHALL EXCAVATE DRAINAGE TRENCHES TO FOLLOW PROPOSED STORM SEWER ALIGNMENTS.
8. GRADES SHOWN ARE FINISHED GRADES. CONTRACTOR SHALL ROUGH GRADE TO SUBGRADE ELEVATION AND LEAVE STREET READY FOR SUBBASE.
9. ALL EXCESS MATERIAL, BITUMINOUS SURFACING, CONCRETE ITEMS, ANY ABANDONED UTILITY ITEMS, AND OTHER UNSTABLE MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OFF THE CONSTRUCTION SITE.
10. REFER TO THE UTILITY PLAN FOR SANITARY SEWER MAIN, WATER MAIN SERVICE LAYOUT AND ELEVATIONS AND CASTING / STRUCTURE NOTATION.
11. CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION OF PAVEMENTS AND CURB AND GUTTER WITH SMOOTH UNIFORM SLOPES TO PROVIDE POSITIVE DRAINAGE.
12. INSTALL A MINIMUM OF 4" CLASS 5 AGGREGATE BASE UNDER CURB AND GUTTER AND CONCRETE SIDEWALKS.
13. UPON COMPLETION OF EXCAVATION AND FILLING, CONTRACTOR SHALL RESTORE ALL STREETS AND DISTURBED AREAS ON SITE. ALL DISTURBED AREAS SHALL BE REVEGETATED WITH A MINIMUM OF 6" OF TOPSOIL.
14. ALL SPOT ELEVATIONS/CONTOURS ARE TO GUTTER / FLOW LINE UNLESS OTHERWISE NOTED.
15. GRADING FOR ALL SIDEWALKS AND ACCESSIBLE ROUTES INCLUDING CROSSING DRIVEWAYS SHALL CONFORM TO CURRENT ADA STATE/NATIONAL STANDARDS. IN NO CASE SHALL ACCESSIBLE RAMP SLOPES EXCEED 1% VERTICAL TO 12% HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPES EXCEED 2%. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPES EXCEED 5%. IN NO CASE SHALL ACCESSIBLE PARKING STALLS OR AISLES EXCEED 2% (1.5% TARGET) IN ALL DIRECTIONS. SIDEWALK ACCESS TO EXTERNAL BUILDING DOORS AND GATES SHALL BE ADA COMPLIANT. CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY IF ADA CRITERIA CANNOT BE MET IN ANY LOCATION PRIOR TO PAVING. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR A.D.A COMPLIANCE ISSUES.
16. MAINTAIN A MINIMUM OF 0.5% GUTTER SLOPE TOWARDS LOW POINTS.
17. CONTRACTOR TO PROVIDE 3" INSULATION BY 5" WIDE CENTERED ON STORM PIPE IF LESS THAN 4" OF COVER IN PAVEMENT AREAS AND LESS THAN 3" OF COVER IN LANDSCAPE AREAS.
18. ROOF DRAIN INVERT CONNECTIONS AT THE BUILDING SHALL BE AT ELEVATION <XXX> OR LOWER UNLESS NOTED OTHERWISE. REFERENCE MEP PLANS FOR ROOF DRAIN CONNECTION.
19. ALL STORM SEWER CONNECTIONS SHALL BE GASKETED AND WATER TIGHT INCLUDING MANHOLE CONNECTIONS.
20. ALL STORM SEWER PIPE SHALL BE AIR TESTED IN ACCORDANCE WITH THE CURRENT PLUMBING CODE.
21. MAINTAIN A MINIMUM OF 1.25% SLOPE IN BITUMINOUS PAVEMENT AREAS, 0.5% SLOPE IN CONCRETE PAVEMENT AREAS.
22. CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "INFALL CURB" WHERE PAVEMENT DRAINS TOWARD GUTTER, AND "OUTFALL" CURB WHERE PAVEMENT DRAINS AWAY FROM GUTTER.





LEGEND

- PROPERTY LINE
- PROPOSED STORM MANHOLE (SOLID CASTING)
- PROPOSED STORM MANHOLE (ROUND INLET CASTING)
- PROPOSED STORM MANHOLE CATCH BASIN (CURB INLET CASTING)
- PROPOSED STORM SEWER CLENOT
- PROPOSED FLARED END SECTION
- PROPOSED RFRAP
- PROPOSED STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED STORM SEWER
- PROPOSED FINGER DRAIN - INSTALL AROUND CATCH BASINS, 4 DRAINS, SPACED AT 90°, EXTENDING 10' FROM CATCH BASIN
- PROPOSED WATERMAIN
- PROPOSED SANITARY SEWER
- PROPOSED UNDERGROUND STORMWATER PIPE DETENTION

GRADING PLAN NOTES

- ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CITY OF HOPKINS, SPECIFICATIONS AND BUILDING PERMIT REQUIREMENTS.
- CONTRACTOR TO CALL GOPHER STATE CALL ONE @ 1-800-252-1166 AT LEAST TWO WORKING DAYS PRIOR TO EXCAVATION/CONSTRUCTION FOR UTILITY LOCATIONS.
- STORM SEWER PIPE SHALL BE AS FOLLOWS:
RCP PER ASTM C-78
HDPE 12" OR GREATER PER ASTM F-2306
PVC SCH. 40 PER ASTM D-3034
STORM SEWER FITTINGS SHALL BE AS FOLLOWS:
RCP PER ASTM C-311, JOINTS PER ASTM C-301, C-900, AND C-443
PVC PER ASTM D-3034, JOINTS PER ASTM D-3212
- CONTRACTOR TO FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO THE START OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER OF ANY DISCREPANCIES OR VARIATIONS.
- SUBGRADE EXCAVATION SHALL BE BACKFILLED IMMEDIATELY AFTER EXCAVATION TO HELP OFFSET ANY STABILITY PROBLEMS DUE TO WATER SEEPAGE OR STEEP SLOPES. WHEN PLACING NEW SURFACE MATERIAL ADJACENT TO EXISTING PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING OF EXISTING PAVEMENT.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HORIZONTAL AND VERTICAL CONTROL.
- CONTRACTOR SHALL EXCAVATE DRAINAGE TRENCHES TO FOLLOW PROPOSED STORM SEWER ALIGNMENTS.
- GRADES SHOWN ARE FINISHED GRADES. CONTRACTOR SHALL ROUGH GRADE TO SUBGRADE ELEVATION AND LEAVE STREET READY FOR SUBGRADE.
- ALL EXCESS MATERIAL, BITUMINOUS SURFACING, CONCRETE ITEMS, ANY ABANDONED UTILITY ITEMS, AND OTHER UNSTABLE MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF THE CONSTRUCTION SITE.
- REFER TO THE UTILITY PLAN FOR SANITARY SEWER MAIN, WATER MAIN SERVICE LAYOUT AND ELEVATIONS AND CASTING / STRUCTURE NOTATION.
- CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION OF PAVEMENTS AND CURB AND GUTTER WITH SMOOTH UNIFORM SLOPES TO PROVIDE POSITIVE DRAINAGE.
- INSTALL A MINIMUM OF 4" CLASS 5 AGGREGATE BASE UNDER CURB AND GUTTER AND CONCRETE SIDEWALKS.
- UPON COMPLETION OF EXCAVATION AND FILLING, CONTRACTOR SHALL RESTORE ALL STREETS AND DISTURBED AREAS ON SITE. ALL DISTURBED AREAS SHALL BE REVEGETATED WITH A MINIMUM OF 6" OF TOPSOIL.
- ALL SPOT ELEVATIONS/CONTOURS ARE TO GUTTER / FLOW LINE UNLESS OTHERWISE NOTED.
- GRADING FOR ALL SIDEWALKS AND ACCESSIBLE ROUTES INCLUDING CROSSING DRIVEWAYS SHALL CONFORM TO CURRENT ADA STATEMENTAL STANDARDS. IN NO CASE SHALL ACCESSIBLE RAMP SLOPES EXCEED 1 VERTICAL TO 12 HORIZONTAL. IN NO CASE SHALL SIDEWALK CROSS SLOPES EXCEED 2%. IN NO CASE SHALL LONGITUDINAL SIDEWALK SLOPES EXCEED 5%. IN NO CASE SHALL ACCESSIBLE PARKING STALLS OR AISLES EXCEED 2% (1.5% TARGET) IN ALL DIRECTIONS. SIDEWALK ACCESS TO EXTERNAL BUILDING DOORS AND GATES SHALL BE ADA COMPLIANT. CONTRACTOR SHALL NOTIFY ENGINEER IMMEDIATELY IF ADA CRITERIA CANNOT BE MET IN ANY LOCATION PRIOR TO PAVING. NO CONTRACTOR CHANGE ORDERS WILL BE ACCEPTED FOR ADA COMPLIANCE ISSUES.
- MAINTAIN A MINIMUM OF 0.5% GUTTER SLOPE TOWARDS LOW POINTS.
- CONTRACTOR TO PROVIDE 3" INSULATION BY 5" WIDE CENTERED ON STORM PIPE IF LESS THAN 4" OF COVER IN PAVEMENT AREAS AND LESS THAN 3" OF COVER IN LANDSCAPE AREAS.
- ROOF DRAIN INVERT CONNECTIONS AT THE BUILDING SHALL BE AT ELEVATION -XXX-XX- OR LOWER UNLESS NOTED OTHERWISE. REFERENCE MEP PLANS FOR ROOF DRAIN CONNECTION.
- ALL STORM SEWER CONNECTIONS SHALL BE GASKETED AND WATER TIGHT INCLUDING MANHOLE CONNECTIONS.
- ALL STORM SEWER PIPE SHALL BE AIR TESTED IN ACCORDANCE WITH THE CURRENT PLUMBING CODE.
- MAINTAIN A MINIMUM OF 1.25% SLOPE IN BITUMINOUS PAVEMENT AREAS, 0.5% SLOPE IN CONCRETE PAVEMENT AREAS.
- CONTRACTOR SHALL REVIEW PAVEMENT GRADIENT AND CONSTRUCT "INFALL CURB" WHERE PAVEMENT DRAINS TOWARD GUTTER, AND "OUTFALL" CURB WHERE PAVEMENT DRAINS AWAY FROM GUTTER.

**BLAKE STATION
SITE B**

Hopkins, MN

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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed engineer under the laws of the State of Minnesota

Signature
WILLIAM D. MATZEK
Typed or Printed Name
42661 XX/XX/XXXX
License # Date

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STORMWATER SUMMARY - BUILDING B PARCEL			
BLAKE & EXCELSIOR - SITE B			
PROPERTY AREA (SITE B)	94,332 SF (2.17 AC)		
PROPOSED SITE IMPERVIOUS AREA	86,096 SF (1.98 AC)		
TRIBUTARY AREA TO BMP 2A/B	61,131 SF (1.40 AC)		
TRIBUTARY AREA TO BMP 2C/D	23,557 SF (0.54 AC)		
DISTURBED AREA	95,876 SF (2.20 AC)		
RATE ATTENUATION SUMMARY			
	1-YEAR (CFS)	10-YEAR (CFS)	100-YEAR (CFS)
BLAKE ROAD S.	EXISTING 3.63	6.60	11.77
	PROPOSED 1.23	4.40	7.97
EXCELSIOR BOULEVARD	EXISTING 3.35	5.90	10.37
	PROPOSED 1.22	5.61	7.88
TOTAL	EXISTING 6.98	12.49	22.15
	PROPOSED 2.35	9.27	15.08
RUNOFF VOLUME SUMMARY			
REQUIRED TREATMENT VOLUME: MOWD REQUIRES A VOLUME ABSTRACTION OF 1" OVER THE IMPERVIOUS AREA (7,175 CF) OR TREATMENT OF THE FLOW RATE RESULTING FROM THE 2" EVENT OVER THE IMPERVIOUS AREA FOR FILTRATION PRACTICES.	14,350 CF		
PROVIDED TREATMENT VOLUME: TO DEMONSTRATE THE UNDERGROUND DETENTION AND TREATMENT VAULT HAVE SUFFICIENT CAPACITY, THE 2" EVENT IS MODELED WITH A 2.55" RAINFALL DEPTH THAT RESULTS IN THE PROVIDED TREATMENT VOLUME.	14,680 CF		
WATER QUALITY SUMMARY			
REQUIRED TSS REMOVAL	70%		
SITE CAPTURE	88.33%		
BMP 2B/2D TSS REMOVAL RATE	80%		
BMP 2B DISCHARGE RATE/TREATMENT CAPACITY	1.24 CFS/1.30 CFS		
BMP 2D DISCHARGE RATE/TREATMENT CAPACITY	0.72 CFS/0.85 CFS		
PROVIDED TSS REMOVAL	70.66% (88.33% SITE CAPTURE + 80% REMOVAL)		

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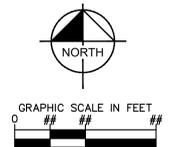
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KEY PLAN

BLAKE ROAD STATION -
SITE B

STORM SEWER PLAN -
SITE B

C503

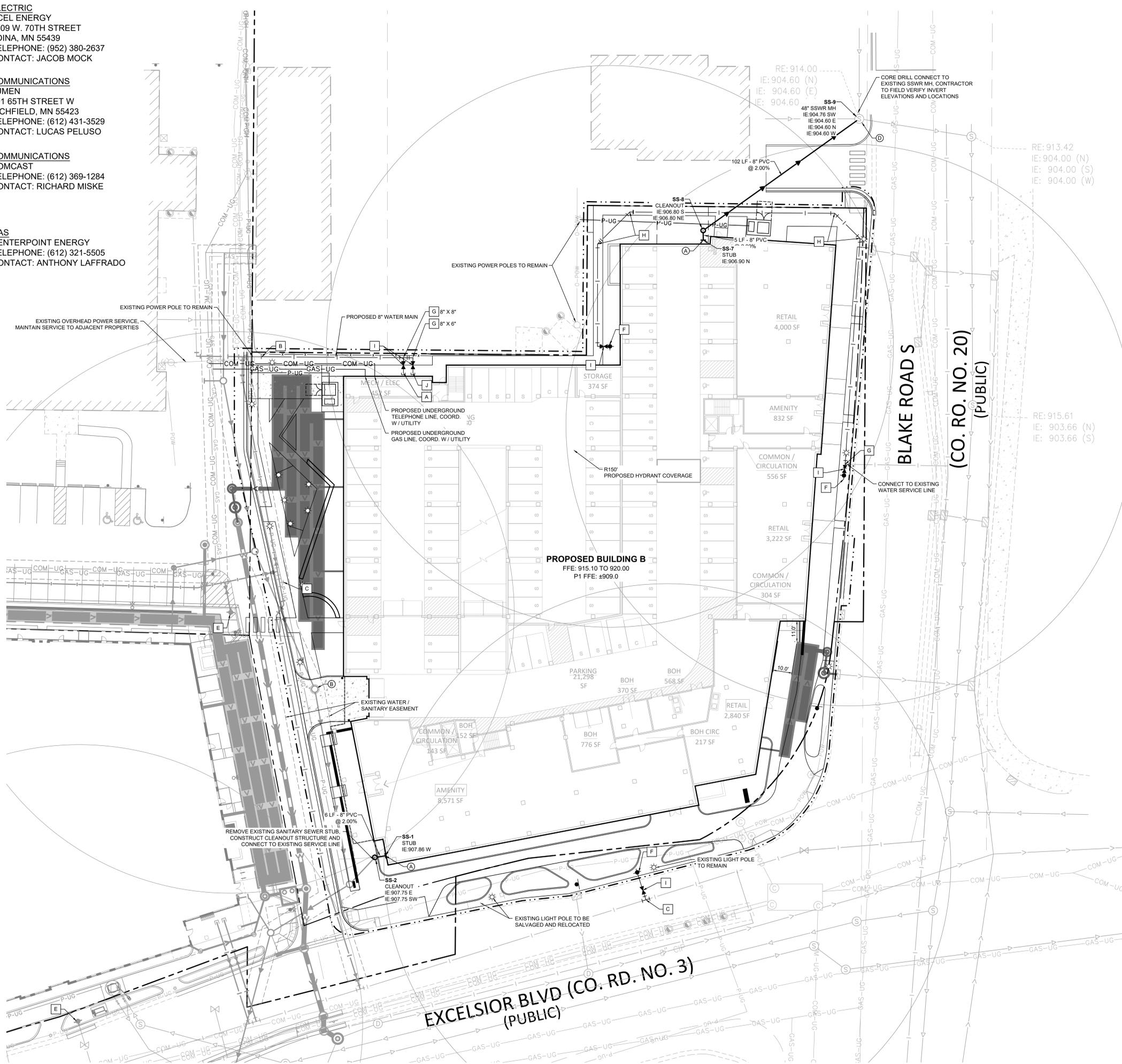


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GAS
CENTERPOINT ENERGY
 TELEPHONE: (612) 321-5505
 CONTACT: ANTHONY LAFFRADO



LEGEND		
EXISTING	PROPOSED	
		GATE VALVE
		HYDRANT
		REDUCER
		TEE
		SANITARY SEWER MANHOLE
		SANITARY CLEANOUT
		WATERMAIN
		SANITARY SEWER
		STORM SEWER
		STORM SEWER
		UNDERGROUND ELECTRIC
		TELEPHONE
		GAS MAIN
		PROPOSED UNDERGROUND STORMWATER PIPE DETENTION
		FULL DEPTH SAWCUT

- UTILITY PLAN NOTES**
- ALL FILL MATERIAL IS TO BE IN PLACE, AND COMPACTED BEFORE INSTALLATION OF PROPOSED UTILITIES.
 - SANITARY SEWER PIPE SHALL BE AS FOLLOWS:
 8" PVC SDR35 PER ASTM D-3034, FOR PIPES LESS THAN 12' DEEP
 12" PVC SDR35 PER ASTM D-3034, FOR PIPES MORE THAN 12' DEEP
 8" PVC SCHEDULE 40 PER ASTM D-3034
 DUCTILE IRON PIPE PER AWWA C150
 - WATER LINES SHALL BE AS FOLLOWS:
 8" AND LARGER: PVC C-900 PER ASTM D 2241
 CLASS 200 UNDER COUNTY ROADS, OTHERWISE CLASS 150
 4" AND LARGER DUCTILE IRON PIPE PER AWWA C150
 SMALLER THAN 4" PIPING SHALL BE COPPER TUBE TYPE "K" PER ANSI B16.22
 WATER LINES SHALL BE INSTALLED IN STRICT ACCORDANCE WITH THE MOST RECENT EDITION OF SEAN SPECIFICATIONS
 - MINIMUM TRENCH WIDTH SHALL BE 2 FEET.
 - ALL WATER JOINTS ARE TO BE MECHANICAL JOINTS WITH RESTRAINTS SUCH AS THRUST BLOCKING, WITH STAINLESS STEEL OR CORAL BLUE BOLTS, OR AS INDICATED IN THE CITY SPECIFICATIONS AND PROJECT DOCUMENTS.
 - ALL UTILITIES SHOULD BE KEPT TEN (10) APART (PARALLEL) OR WHEN CROSSING 18" VERTICAL CLEARANCE (OUTSIDE EDGE OF PIPE TO OUTSIDE EDGE OF PIPE OR STRUCTURE).
 - CONTRACTOR SHALL MAINTAIN A MINIMUM OF 7'-0" COVER ON ALL WATERLINES.
 - IN THE EVENT OF A VERTICAL CONFLICT BETWEEN WATER LINES, SANITARY LINES, STORM LINES AND GAS LINES, OR ANY OBSTRUCTION (EXISTING AND PROPOSED), THE SANITARY LINE SHALL BE 8" OR 9" ABOVE WITH MECHANICAL JOINTS AT LEAST 10 FEET ON EITHER SIDE OF THE CENTER LINE OF THE OBSTRUCTION. THE WATER LINE SHALL HAVE MECHANICAL JOINTS WITH APPROPRIATE FASTENERS AS REQUIRED TO PROVIDE A MINIMUM OF 18" VERTICAL SEPARATION, MEETING REQUIREMENTS OF ANSI A21.10 OR ANSI 11.11 (AWWA C-151) (CLASS 80).
 - LINES UNDERGROUND SHALL BE INSTALLED, INSPECTED AND APPROVED BEFORE BACKFILLING.
 - TOPS OF MANHOLES SHALL BE RAISED AS NECESSARY TO BE FLUSH WITH PROPOSED PAVEMENT ELEVATIONS, AND TO BE ONE FOOT ABOVE FINISHED GROUND ELEVATIONS, IN GREEN AREAS, WITH WATERFIGHT LIDS.
 - ALL CONCRETE FOR ENCASEMENTS SHALL HAVE A MINIMUM 28 DAY COMPRESSION STRENGTH AT 3000 P.S.I.
 - EXISTING UTILITIES SHALL BE VERIFIED IN FIELD PRIOR TO INSTALLATION OF ANY NEW LINES.
 - REFER TO INTERIOR PLUMBING DRAWINGS FOR TIE-IN OF ALL UTILITIES.
 - CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE SPECIFICATIONS OF THE CITY OF HOPKINS AND/OR STATE OF MINNESOTA WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER AND SEWER LINES.
 - THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANIES AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES WHICH CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THE PLANS.
 - CONTRACTOR IS RESPONSIBLE FOR ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED BY CODES AND/OR UTILITY SERVICE COMPANIES.
 - CONTRACTOR SHALL COORDINATE WITH ALL UTILITY COMPANIES FOR INSTALLATION REQUIREMENTS AND SPECIFICATIONS.
 - CONTRACTOR SHALL REFERENCE ARCH / MEP PLANS FOR SITE LIGHTING AND ELECTRICAL PLAN.
 - BACKFLOW DEVICES (DDCV AND PRZ ASSEMBLIES) AND METERS ARE LOCATED IN THE INTERIOR OF THE BUILDING. REF. ARCH / MEP PLANS.
 - ALL ONSITE WATERMANS AND SANITARY SEWERS SHALL BE PRIVATELY OWNED AND MAINTAINED.
 - ALL WATERMAIN STUBOUTS SHALL BE MECHANICALLY RESTRAINED WITH REACTION BLOCKING.
 - THE WATER DISTRIBUTION SYSTEM SHALL BE DISINFECTED IN ACCORDANCE WITH MN PLUMBING CODE RULES PER SECTION 609.9.A
 - THE PLUMBING SYSTEM SHALL BE TESTED IN ACCORDANCE WITH MN PLUMBING CODE RULES PER SECTIONS 609.4 AND 712.0. THIS SHALL INCLUDE THE STORM SEWER LOCATED WITHIN 10 FEET OF THE BUILDING AND WITHIN 10 FEET OF THE BURIED WATER SUPPLY.
 - ALL PORTIONS OF THE STORM SEWER SYSTEM LOCATED WITHIN 10 FEET OF THE BUILDING OR WATER SERVICE LINE MUST BE TESTED IN ACCORDANCE WITH MINNESOTA RULES, CHAPTER 474, SECTION 1109.
 - ALL STORM PIPE ENTERING STRUCTURES SHALL BE WATER TIGHT OR BE OF FLEXIBLE COMPRESSION JOINTS LOCATED BETWEEN 12 AND 36 INCHES FROM MANHOLE JOINT CONNECTIONS SHALL BE FLEXIBLE WATER STOPS, RESILIENT CONNECTORS, OR OTHER FLEXIBLE SYSTEMS APPROVED BY THE ENGINEER TO MAKE WATER TIGHT CONNECTION TO MANHOLES AND OTHER STRUCTURES UNLESS OTHERWISE STATED BY CITY AND STATE DESIGN STANDARDS AND SPECIFICATIONS. CEMENT MORTAR JOINTS ARE NOT APPROVED.

SANITARY SEWER KEYNOTE LEGEND

(A) SANITARY SEWER SERVICE, CONNECT TO BUILDING PLUMBING COORDINATE W/ MEP PLANS

(B) ADJUST EXISTING CASTING TO NEW RIM ELEVATION

(C) CONSTRUCT MANHOLE OVER EXISTING SANITARY SEWER, CONNECT TO EXISTING SERVICE PIPE, FIELD VERIFY LOCATION AND ELEVATION.

(D) CORE DRILL AND CONNECT TO EXISTING STRUCTURE, FIELD VERIFY LOCATION AND ELEVATION

WATERMAIN KEYNOTE LEGEND

(A) PROPOSED 8" FIRE WATER SERVICE, PROVIDE BACKFLOW PREVENTER INSIDE BUILDING

(B) CONNECT TO EXISTING WATER STUB

(C) CAP EXISTING TEE TO THE EAST AFTER REMOVING EXISTING WATER SERVICE LINE

(D) CONNECT TO EXISTING WATER LINE TO MAINTAIN SERVICE

(E) EXISTING HYDRANT (SITE A)

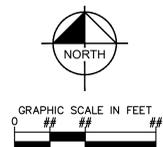
(F) PROPOSED FIRE HYDRANT AND GATE VALVE ASSEMBLY, PROVIDE 6" LEAD

(G) TEE

(H) 45-DEGREE BEND

(I) GATE VALVE

(J) PROPOSED 6" DOMESTIC WATER SERVICE



**BLAKE STATION
 SITE B**

Hopkins, MN



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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed engineer under the laws of the State of Minnesota

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 42661 XX/XX/XXXX
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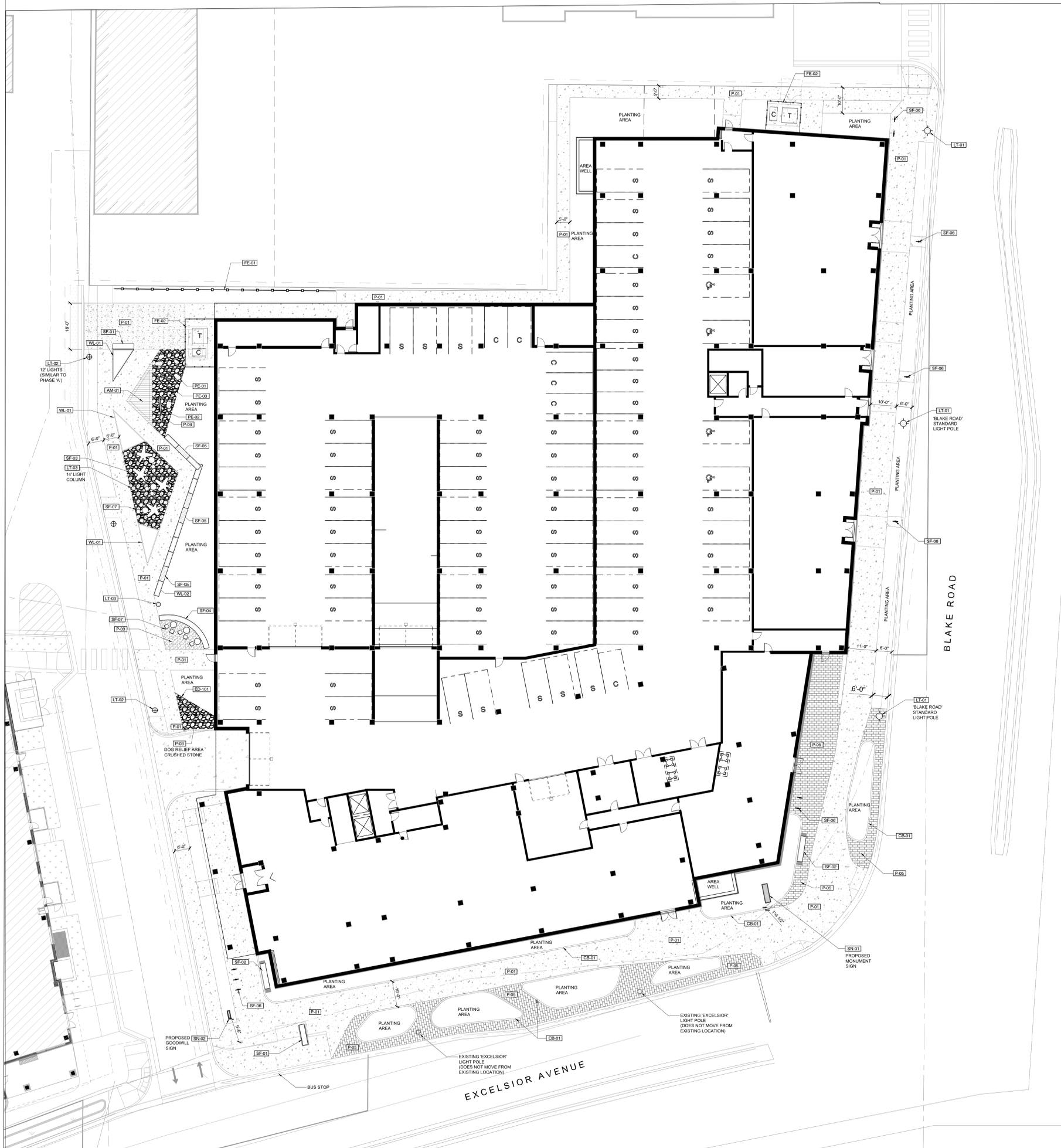
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KEY PLAN

BLAKE ROAD STATION -
 SITE B

UTILITY PLAN - SITE B

C601



SYMBOL	DESCRIPTION	QTY	MATERIAL PROFILE/ASSEMBLY	MANUFACTURER	PRODUCT/MODEL
[AM-01]	WOOD DECKMINI STAGE	128 SF	TBD		
[CB-01]	CONCRETE PLANTER CURB	621 LF	CUSTOM		CONCRETE CURBING
[ED-01]	STEEL EDGING	46 LF	3/16" GALVANIZED STEEL	RYERSON	3/16" GALVANIZED STEEL
[FE-01]	8' HEIGHT WOODEN FENCE	244 LF			
[FE-02]	UTILITY SCREEN FENCE	235 LF			MATCH PHASE 1 DESIGN
[LT-01]	EXCELSIOR STREET LIGHT	3			
[LT-02]	PEDESTRIAN LIGHT	3			
[LT-03]	PEDESTRIAN COLUMN LIGHT	6			
[P-01]	CONCRETE PAVING	14,738 SF	4" CONCRETE PAVEMENT OVER 6" COMPACTED AGGREGATE BASE, COMPACTED SUBGRADE		
[P-02]	CONCRETE PAVING - VEHICULAR	703 SF	6" REINFORCED VEHICULAR RATED CONCRETE PAVEMENT, OVER COMPACTED AGGREGATE BASE, COMPACTED SUBGRADE		
[P-03]	STABILIZED CRUSHED STONE	662 SF	3" DEPTH 3/4" DECOMPOSED GRANITE, STABILIZED, OVER COMPACTED AGGREGATE BASE OVER COMPACTED SUBGRADE		
[P-04]	PLAY AREA SURFACE	305 SF	ENGINEERED WOOD FIBER, DEPTH PER ASTM STND. WITH SUBSURFACE DRAINAGE SYSTEM		
[P-05]	CONCRETE UNIT PAVER	2,660 SF	TBD		
[PE-01]	PLAY BOULDER	5			
[PE-02]	BALANCE BEAM	1	10' 2" X 8" X 1' 3"	KOMPAN	NRO801-0801
[PE-03]	STILTS	1	4' 2" X 8" X 1' 3" 7"	KOMPAN	NRO806-0801
[SF-01]	LINEAR BENCH - 8'	2	2' X 8' LINEAR BENCH	ANNOVA	INF24L8T
[SF-02]	LINEAR BENCH - 10'	2	2' X 10' LINEAR BENCH	ANNOVA	INF24L10T
[SF-03]	CAST CONCRETE BENCH	4	REINFORCED CAST STONE BENCH, 75" X 60" X 12"	LANDSCAPE FORMS	FLOR-5M
[SF-04]	CURVED BENCH	1	2' CURVED BENCH 16' RADIUS	ANNOVA	INF24C16R0T
[SF-05]	GABION BENCH	4	2' X 6' GABION BENCH, FILL W/ DRESSOR TRAP ROCK	ANNOVA	CE18T2T
[SF-06]	BIKE RACK	10		FORMS + SURFACES	TRIO BIKE RACK
[SF-07]	MOVABLE CHAIR	11	TBD		
[SF-08]	MOVABLE TABLE	6	TBD		
[SN-01]	MONUMENT SIGN	1	TBD		
[SN-02]	GOOD WILL SIGN	1	RELOCATED GOOD WILL SIGN		
[WL-01]	STEEL PLANTER WALL	120 LF	16" HEIGHT WEATHERING STEEL PLANTER WALL WITH BENT TOP		

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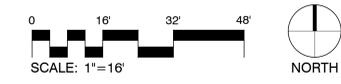
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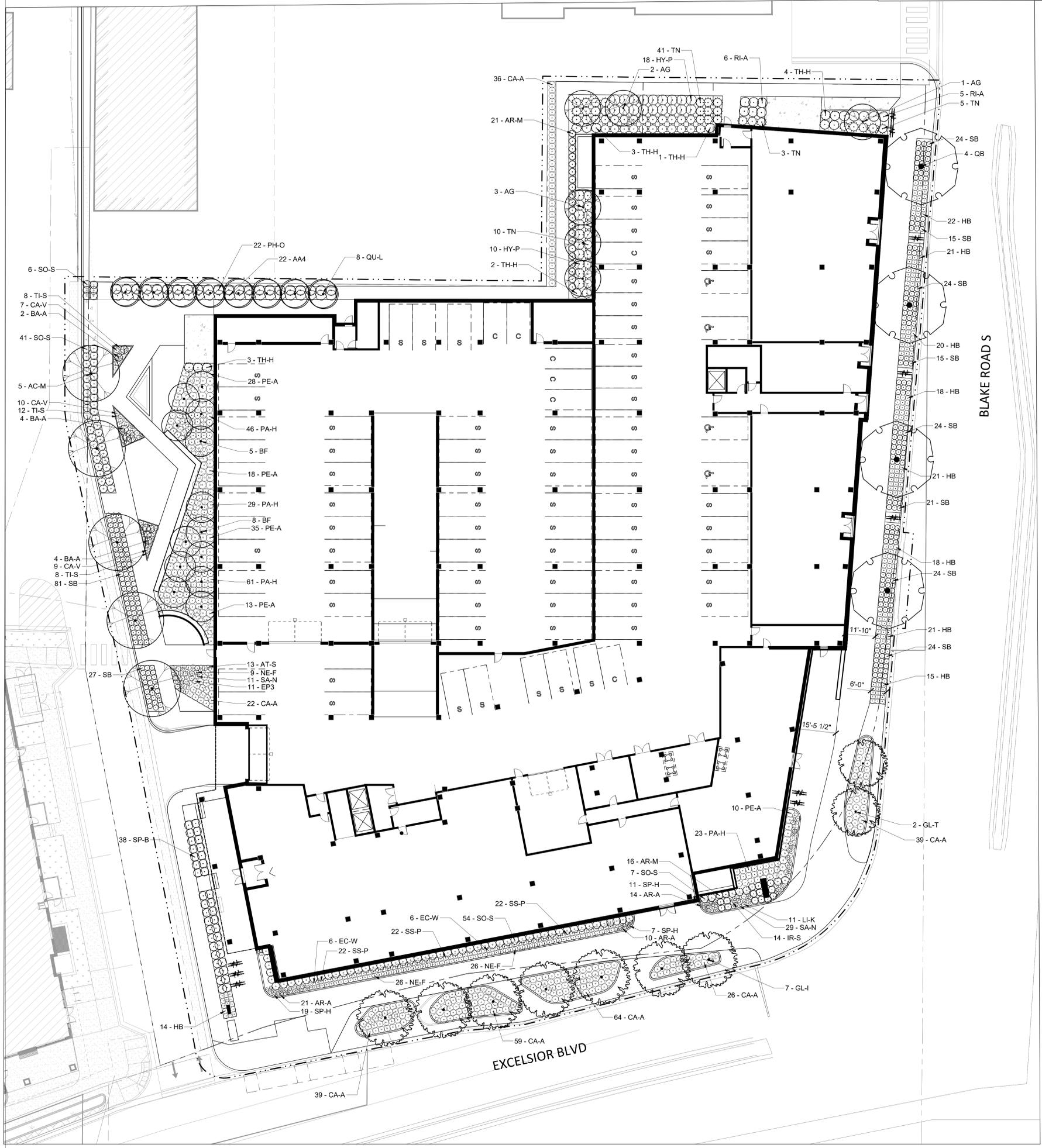
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KEY PLAN

BLAKE ROAD STATION

SITE PLAN - BUILDING B
L111





PLANT SCHEDULE LANDSCAPE			
DECIDUOUS TREES	CODE	QTY	BOTANICAL / COMMON NAME
	AC-M	5	ACER MIYABEI 'MORTON' TM / STATE STREET MIYABEI MAPLE
	BF	13	BETULA PLATYPHYLLA 'FARGO' TM / DAKOTA PINNACLE ASIAN WHITE BIRCH
	GL-T	2	GLEDITSIA TRIACANTHOS DRAVES / HONEY LOCUST STREET KEEPER
	GL-I	7	GLEDITSIA TRIACANTHOS INERMIS 'SKYCOLE' TM / SKYLINE THORNLESS HONEY LOCUST
	QB	4	QUERCUS BICOLOR / SWAMP WHITE OAK
	QU-L	8	QUERCUS X WAREI 'LONG' TM / REGAL PRINCE OAK
ORNAMENTAL TREES	CODE	QTY	BOTANICAL / COMMON NAME
	AG	6	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' / 'AUTUMN BRILLIANCE' SERVICEBERRY
SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME
	AA4	22	ARONIA MELANOCARPA 'AUTUMN MAGIC' / AUTUMN MAGIC BLACK CHOKEBERRY
	AR-M	37	ARONIA MELANOCARPA 'UCONNAM165' / LOW SCAPE MOUND CHOKEBERRY
	HY-P	28	HYDRANGEA PANICULATA 'JANE' TM / LITTLE LIME PANICLE HYDRANGEA
	PH-O	22	PHYSOCARPUS OPULIFOLIUS 'DART'S GOLD' / DART'S GOLD NINEBARK
	RI-A	11	RIBES ALPINUM / ALPINE CURRANT
	SO-S	108	SORBARIA SORBIFOLIA 'SEM' / SEM ASH LEAF SPIREA
	SP-B	38	SPIRAEA BETULIFOLIA 'TOR GOLD' TM / GLOW GIRL BIRCHLEAF SPIREA
	TN	59	TAXUS X MEDIA 'TAUNTON' / TAUNTON YEW
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME
	TH-H	13	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP ARBORVITAE
GRASSES	CODE	QTY	BOTANICAL / COMMON NAME
	CA-A	285	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS
	CA-V	26	CAREX VULPINOIDEA / FOX SEDGE
	PA-H	159	PANICUM VIRGATUM 'HEAVY METAL' / HEAVY METAL SWITCH GRASS
	SS-P	66	SCHIZACHYRIUM SCOPARIUM 'PRAIRIE BLUES' / PRAIRIE BLUES LITTLE BLUESTEM
	SB	279	SCHIZACHYRIUM SCOPARIUM 'BLUE HEAVEN' / BLUE HEAVEN LITTLE BLUESTEM
	SP-H	37	SPOROBOLUS HETEROLEPIS / PRAIRIE DROPSEED
PERENNIALS	CODE	QTY	BOTANICAL / COMMON NAME
	AT-S	13	ALLIUM TANGUTICUM 'SUMMER BEAUTY' / SUMMER BEAUTY GLOBE LILY
	AR-A	45	ARUNCUS AETHUSIFOLIUS / DWARF GOATSBEARD
	BA-A	10	BAPTISIA AUSTRALIS / BLUE WILD INDIGO
	EP3	11	ECHINACEA PALLIDA / PALE PURPLE CONEFLOWER
	EC-W	12	ECHINACEA PURPUREA 'WHITE SWAN' / WHITE SWAN CONEFLOWER
	HB	170	HEMEROCALLIS X 'BAJA' / BAJA DAYLILY
	IR-S	14	IRIS SIBIRICA 'CAESAR'S BROTHER' / CAESAR'S BROTHER SIBERIAN IRIS
	LI-K	11	LIATRIS SPICATA 'KOBOLD' / KOBOLD BLAZING STAR
	NE-F	61	NEPETA X FAASSENII 'WALKER'S LOW' / WALKER'S LOW CATMINT
	PE-A	104	PEROVSKIA ATRIPLICIFOLIA 'BLUE JEAN BABY' / BLUE JEAN BABY RUSSIAN SAGE
	SA-N	40	SALVIA NEMOROSA 'CARDONNA' / CARDONNA MEADOW SAGE
	TI-S	28	TIARELLA X 'SUGAR AND SPICE' / SUGAR AND SPICE FOAMFLOWER

BLAKE STATION
BLDG B
HOPKINS, MN 55343



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Signature
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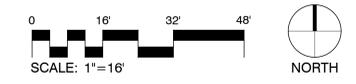
20-172
PROJECT NUMBER

BD DRAWN BY JS CHECKED BY
KEY PLAN

BLAKE ROAD STATION

LANDSCAPE PLAN - BUILDING B

L140

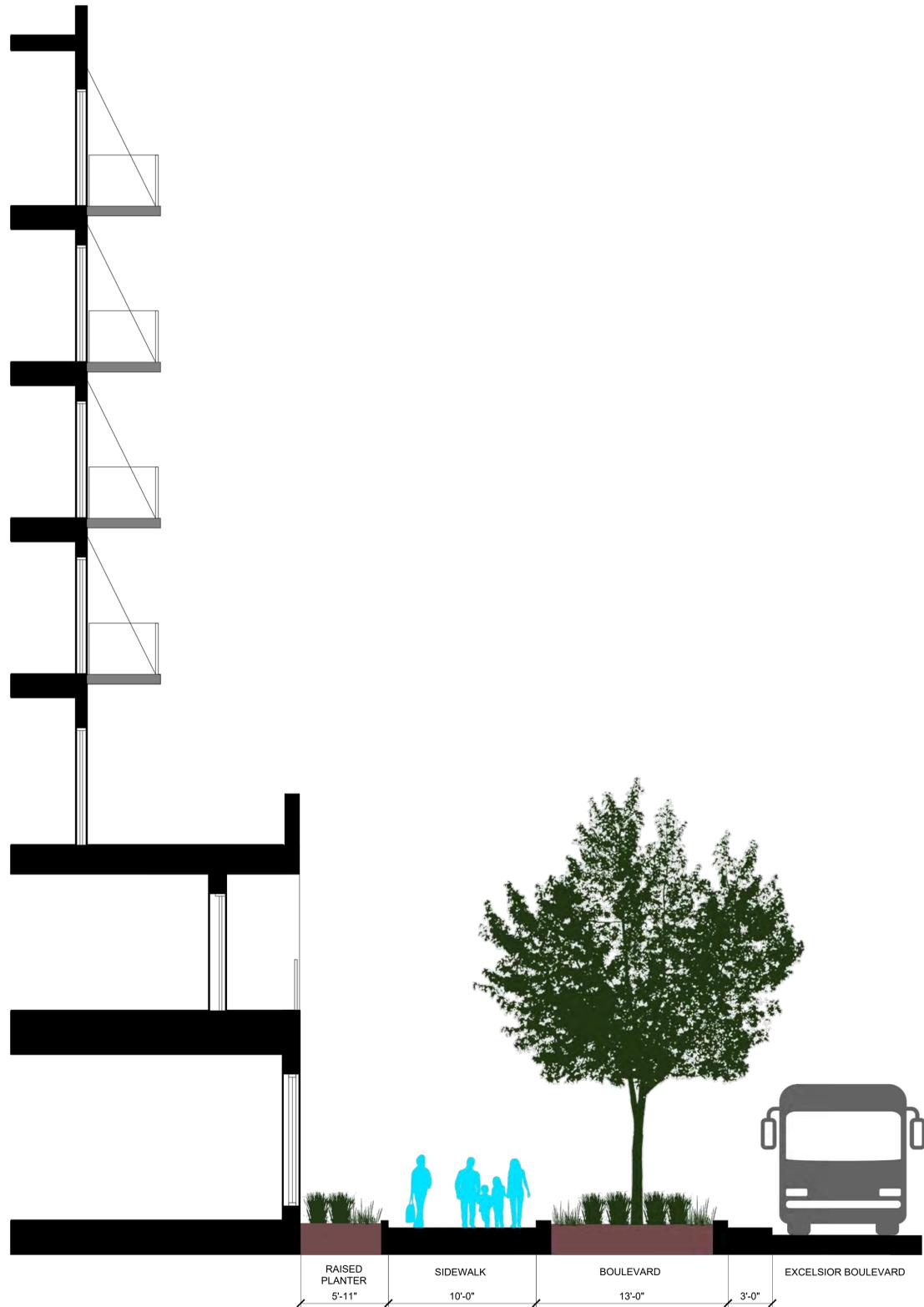


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Signature
Jesse Symonowicz
Typed or Printed Name
26970
License # Date

DF/ DAMON FARBER
LANDSCAPE ARCHITECTS
310 South 4th Avenue, Suite 7050
Minneapolis, MN 55415
p: 612.332.7522

**NOT FOR
CONSTRUCTION**



2 SECTION - STREETScape @ EXCELSIOR
1/4" = 1'-0"



1 SECTION - STREETScape @ BLAKE ROAD
1/4" = 1'-0"

LUA SUBMITTAL
02/18/2022

ORIGINAL ISSUE:

REVISIONS:
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20-172
PROJECT NUMBER

BD JS
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KEY PLAN

BLAKE ROAD STATION

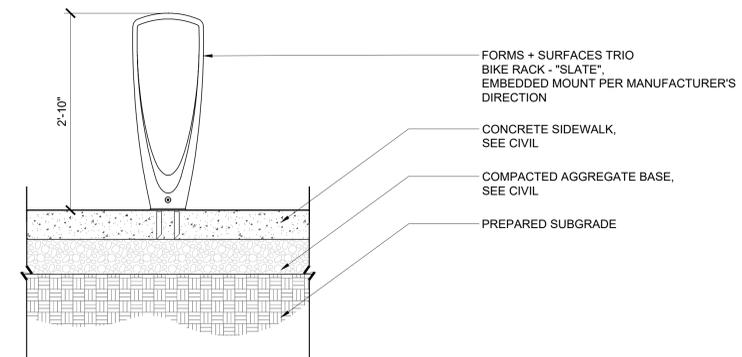
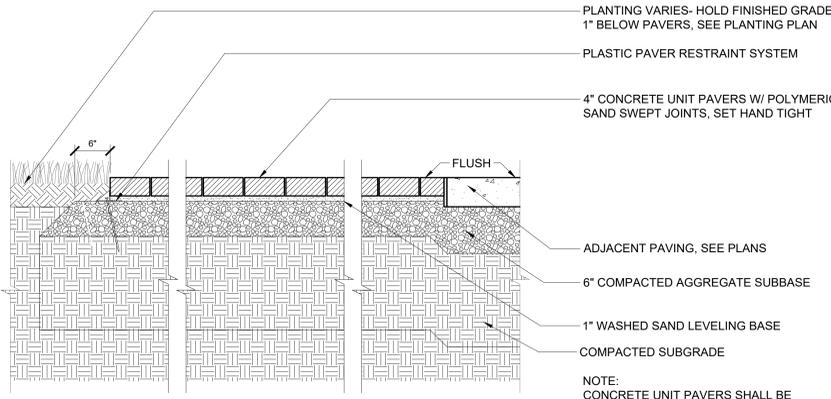
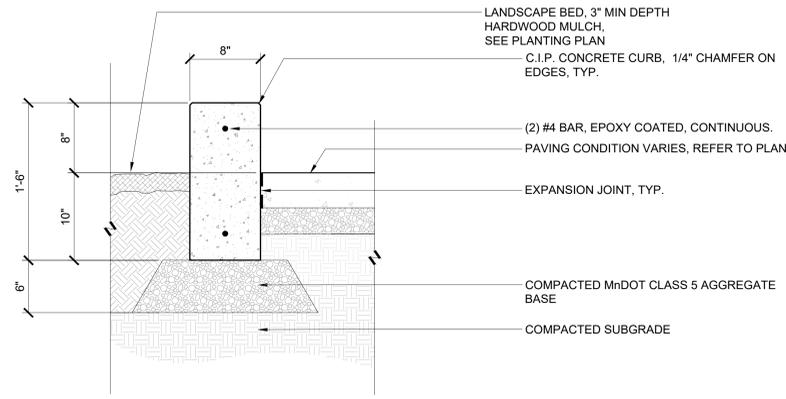
LANDSCAPE SECTIONS
L400

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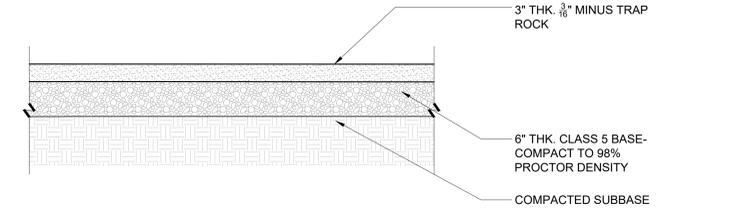
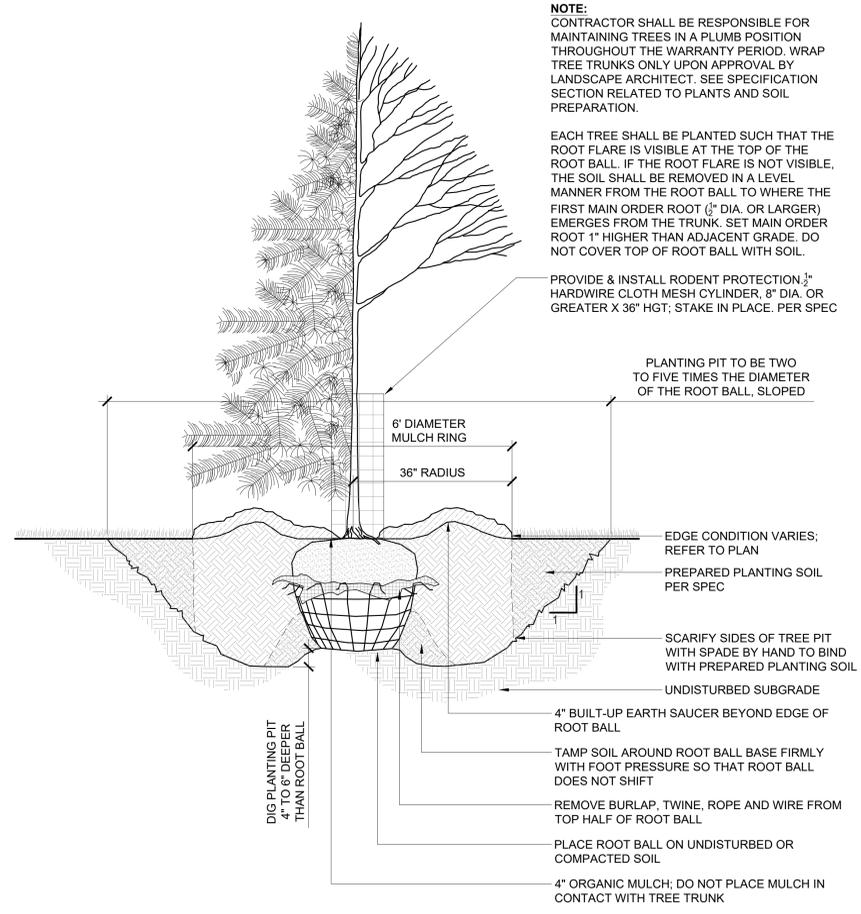
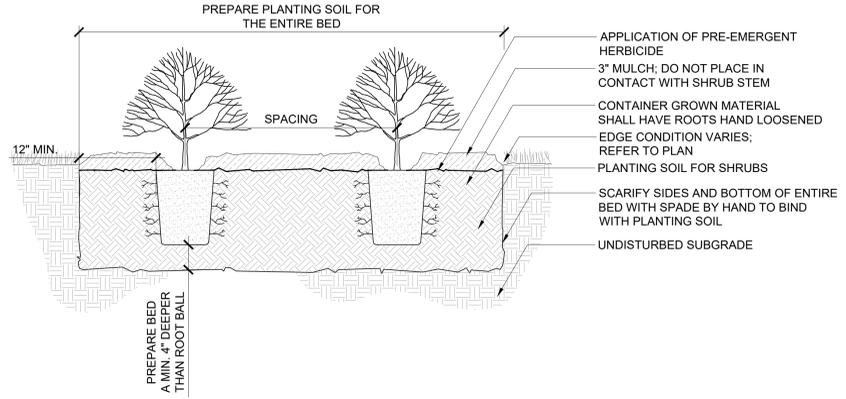
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8 CONCRETE PLANTER CURB
1 1/2" = 1'-0"

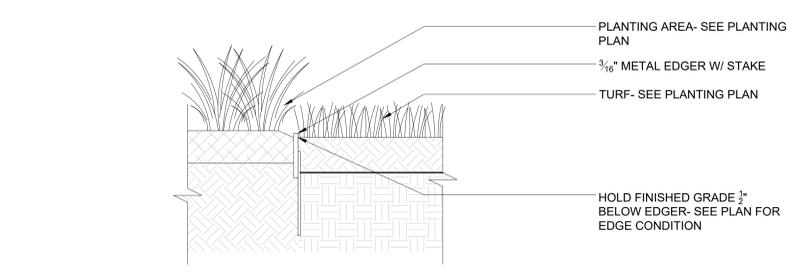
5 CONCRETE UNIT PAVER
1" = 1'-0"

3 BIKE RACK, SURFACE MOUNT
1" = 1'-0"

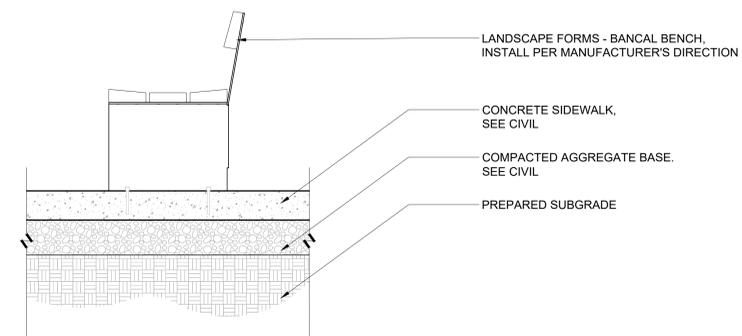


7 DETAIL - SHRUB PLANTING
1" = 1'-0"

2 CRUSHED STONE PAVING
1" = 1'-0"



4 DETAIL - TREE PLANTING
3/4" = 1'-0"



6 STEEL EDGING
1 1/2" = 1'-0"

1 SITE BENCH - INGROUND MOUNT
1" = 1'-0"

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02/18/2022

ORIGINAL ISSUE:

Revisions:	No.	Description	Date

20-172
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BLAKE ROAD STATION

LANDSCAPE DETAILS
L500

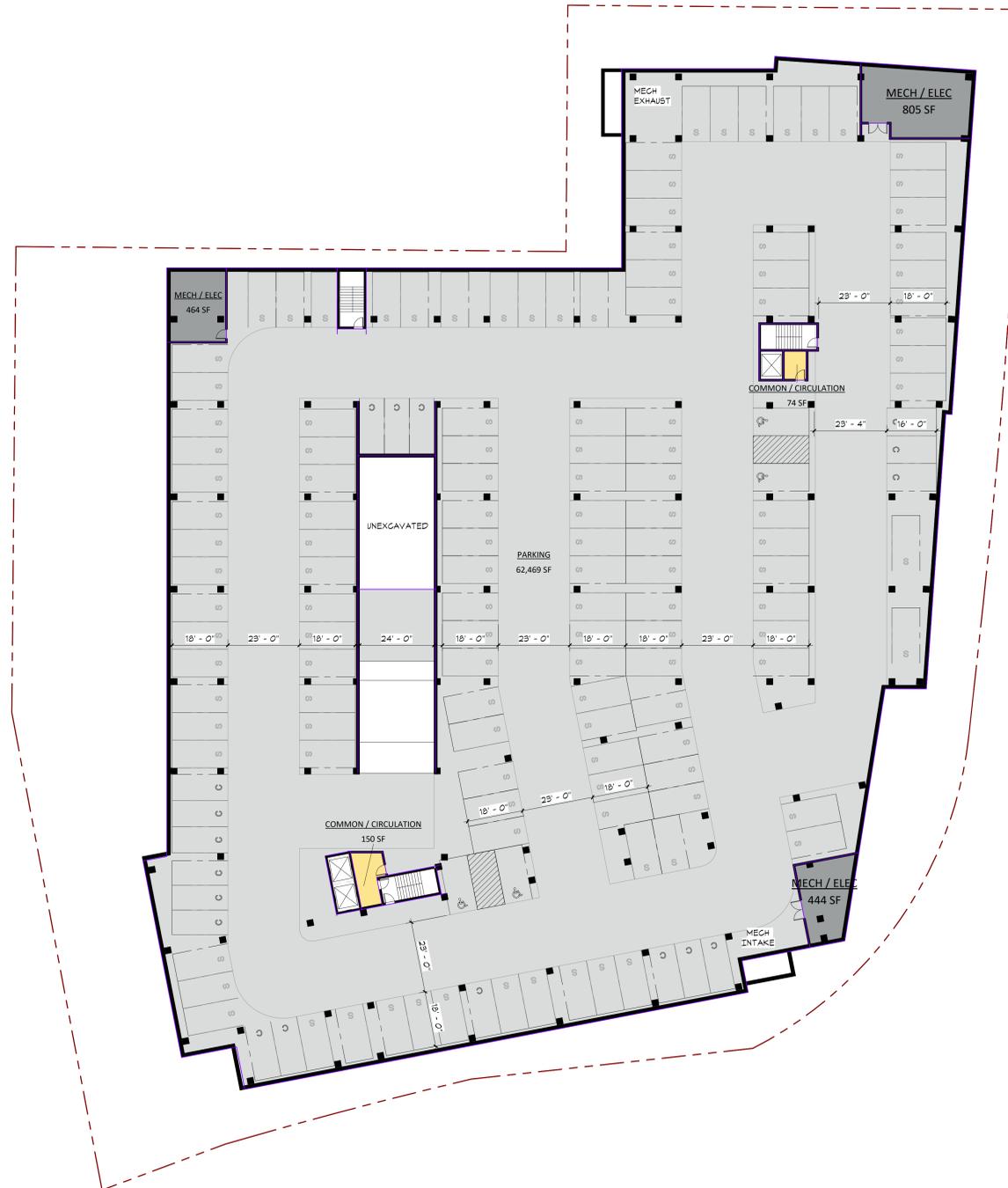
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CONSTRUCTION**



PARKING SCHEDULE - RETAIL			
Type	Level	Count	
LEVEL 1			
8'-6" x 18" - COMPACT	LEVEL 1	1	
8'-6" x 18" - STANDARD	LEVEL 1	3	
9'-0" x 18" - STANDARD	LEVEL 1	33	
9'-0" x 16" - COMPACT	LEVEL 1	1	
9'-0" x 18" - ADA	LEVEL 1	4	
TOTAL STALLS:		42	

PARKING SCHEDULE - APARTMENT			
Type	Level	Count	
LEVEL P1			
8'-0" x 18" - COMPACT	LEVEL P1	7	
8'-6" x 18" - COMPACT	LEVEL P1	8	
9'-0" x 18" - STANDARD	LEVEL P1	126	
9'-0" x 16" - COMPACT	LEVEL P1	3	
9'-0" x 18" - ADA	LEVEL P1	4	
TOTAL STALLS:		148	
LEVEL 1			
8'-0" x 18" - COMPACT	LEVEL 1	3	
9'-0" x 18" - STANDARD	LEVEL 1	38	
9'-0" x 16" - COMPACT	LEVEL 1	2	
9'-0" x 18" - STANDARD	LEVEL 1	1	
TOTAL STALLS:		44	
LEVEL 2			
8'-0" x 18" - COMPACT	LEVEL 2	6	
8'-6" x 18" - COMPACT	LEVEL 2	5	
9'-0" x 18" - STANDARD	LEVEL 2	88	
9'-0" x 16" - COMPACT	LEVEL 2	2	
9'-0" x 18" - ADA	LEVEL 2	4	
TOTAL STALLS:		105	
TOTAL STALLS:		297	

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2/18/2022

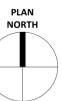
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KEY PLAN



BLAKE STATION SITE B

LEVEL P1 PLAN

A1-0

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DEPARTMENT GROSS AREAS	
Area Department	Area
LEVEL P1	
BOH	1,713 SF
COMMON / CIRCULATION	225 SF
PARKING	62,469 SF
	64,406 SF
LEVEL 1	
AMENITY	9,403 SF
BOH	2,910 SF
COMMON / CIRCULATION	1,080 SF
PARKING	39,478 SF
RETAIL	10,062 SF
	62,933 SF
LEVEL 2	
BOH	1,327 SF
COMMON / CIRCULATION	2,782 SF
PARKING	42,751 SF
RESIDENTIAL	16,471 SF
	63,332 SF
LEVEL 3	
AMENITY	2,722 SF
BOH	646 SF
COMMON / CIRCULATION	3,793 SF
RESIDENTIAL	33,829 SF
	40,991 SF
LEVEL 4	
BOH	648 SF
COMMON / CIRCULATION	3,529 SF
RESIDENTIAL	36,665 SF
	40,842 SF
LEVEL 5	
BOH	648 SF
COMMON / CIRCULATION	3,529 SF
RESIDENTIAL	36,665 SF
	40,842 SF
LEVEL 6	
BOH	648 SF
COMMON / CIRCULATION	3,529 SF
RESIDENTIAL	36,665 SF
	40,842 SF
LEVEL 7	
AMENITY	745 SF
BOH	609 SF
COMMON / CIRCULATION	3,530 SF
RESIDENTIAL	34,603 SF
	39,487 SF
	393,676 SF

PARKING SCHEDULE - APARTMENT		
Type	Level	Count
LEVEL P1		
8'-0" x 18' - COMPACT	LEVEL P1	7
8'-0" x 18' - COMPACT	LEVEL P1	8
9'-0" x 18' - STANDARD	LEVEL P1	126
9'-0" x 16' - COMPACT	LEVEL P1	3
9'-0" x 18' - ADA	LEVEL P1	4
		148
LEVEL 1		
8'-0" x 18' - COMPACT	LEVEL 1	3
9'-0" x 18' - STANDARD	LEVEL 1	38
9'-0" x 16' - COMPACT	LEVEL 1	2
9'-0" x 18' - STANDARD	LEVEL 1	1
		44
LEVEL 2		
8'-0" x 18' - COMPACT	LEVEL 2	6
8'-0" x 18' - COMPACT	LEVEL 2	5
9'-0" x 18' - STANDARD	LEVEL 2	88
9'-0" x 16' - COMPACT	LEVEL 2	2
9'-0" x 18' - ADA	LEVEL 2	4
		105
TOTAL STALLS:		297

PARKING SCHEDULE - RETAIL		
Type	Level	Count
LEVEL 1		
8'-0" x 18' - COMPACT	LEVEL 1	1
8'-0" x 18' - STANDARD	LEVEL 1	3
9'-0" x 18' - STANDARD	LEVEL 1	33
9'-0" x 16' - COMPACT	LEVEL 1	1
9'-0" x 18' - ADA	LEVEL 1	4
TOTAL STALLS:		42

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2/18/2022

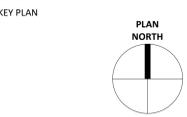
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BLAKE STATION SITE B

LEVEL 1 PLAN
A1-1

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****UNIT MIX SUMMARY - OPTION 1)**

Name	Qty	%	Area	Min - Max SF
1 BED	97	39%	66,839 SF	645 SF ... 720 SF
1 BED + DEN	2	1%	1,572 SF	774 SF ... 797 SF
2 BED	80	32%	81,097 SF	904 SF ... 1,052 SF
3 BED	11	4%	14,087 SF	1,272 SF ... 1,318 SF
ALCOVE	40	16%	21,856 SF	495 SF ... 597 SF
STUDIO	20	8%	9,650 SF	464 SF ... 521 SF
Grand total:	250		194,900 SF	

PARKING SCHEDULE - APARTMENT

Type	Level	Count
LEVEL P1		
8'-0" x 18' - COMPACT	LEVEL P1	7
8'-6" x 18' - COMPACT	LEVEL P1	8
9'-0" x 18' - STANDARD	LEVEL P1	126
9'-0" x 16' - COMPACT	LEVEL P1	3
9'-0" x 18' - ADA	LEVEL P1	4
		148
LEVEL 1		
8'-0" x 18' - COMPACT	LEVEL 1	3
9'-0" x 18' - STANDARD	LEVEL 1	38
9'-0" x 16' - COMPACT	LEVEL 1	2
9'-0" x 18' - STANDARD	LEVEL 1	1
		44
LEVEL 2		
8'-0" x 18' - COMPACT	LEVEL 2	6
8'-6" x 18' - COMPACT	LEVEL 2	5
9'-0" x 18' - STANDARD	LEVEL 2	88
9'-0" x 16' - COMPACT	LEVEL 2	2
9'-0" x 18' - ADA	LEVEL 2	4
		105
TOTAL STALLS:		297

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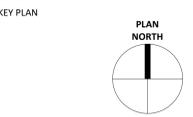
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BLAKE STATION SITE B

LEVEL 2 PLAN
A1-2

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****UNIT MIX SUMMARY - OPTION 1)**

Name	Qty	%	Area	Min - Max SF
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1 BED + DEN	2	1%	1,572 SF	774 SF ... 797 SF
2 BED	80	32%	81,097 SF	904 SF ... 1,052 SF
3 BED	11	4%	14,087 SF	1,272 SF ... 1,318 SF
ALCOVE	40	16%	21,656 SF	495 SF ... 597 SF
STUDIO	20	8%	9,650 SF	464 SF ... 521 SF
Grand total:	250		194,900 SF	



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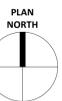
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KEY PLAN



BLAKE STATION SITE B

LEVEL 4-6 PLAN

A1-4

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Name	Qty	%	Area	Min - Max SF
1 BED	97	39%	66,839 SF	645 SF ... 720 SF
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STUDIO	20	8%	9,650 SF	464 SF ... 521 SF
Grand total:	250		194,900 SF	

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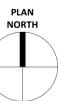
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KEY PLAN



BLAKE STATION SITE B

LEVEL 7 PLAN

A1-5

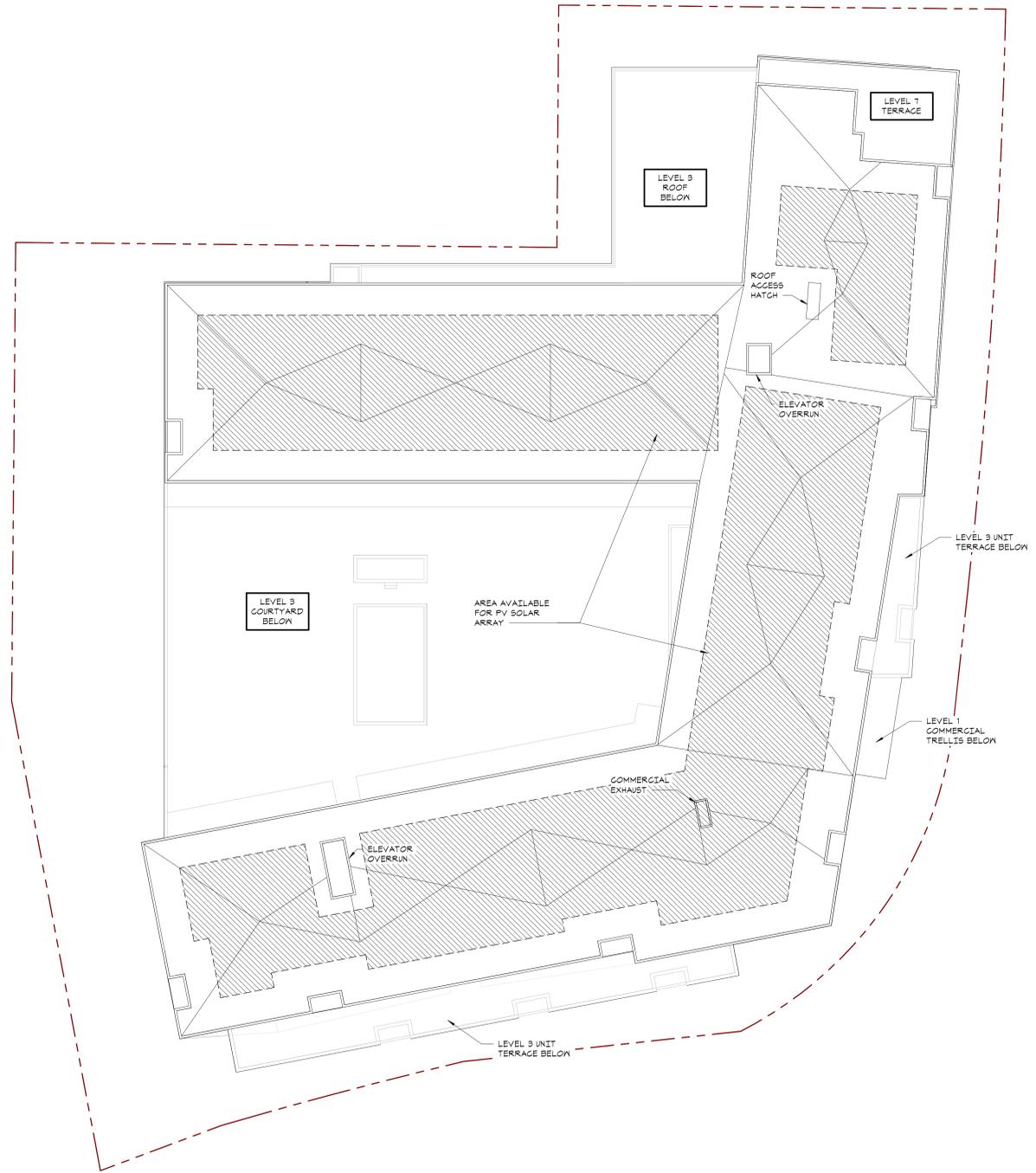
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1 ROOF LEVEL
A1-6 1" = 20'-0"

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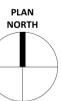
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BLAKE STATION SITE B

ROOF PLAN

A1-6

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VIEW EAST ON EXCELSIOR BOULEVARD



POCKET PARK



COMMERCIAL PATIO ON BLAKE ROAD



VIEW WEST ON EXCELSIOR BOULEVARD

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KEY PLAN

BLAKE STATION SITE B

PERSPECTIVE VIEWS

A3-1

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ELEVATION VIEW	TOTAL AREA	GLASS AREA	GLASS %	BRICK AREA	BRICK %	METAL PANEL AREA	METAL PANEL %	METAL TILE AREA	METAL TILE %	FIBER CEMENT SIDING AREA	FIBER CEMENT SIDING %	TOTAL %
NORTH	21,046	5,851	28%	4,861	23%	1,090	5%	6,545	31%	3,789	18%	100%
WEST	20,330	5,124	25%	6,642	33%	1,833	9%	6,492	32%	2,072	10%	100%
SOUTH	17,538	5,397	31%	3,830	22%	1,030	6%	5,475	31%	2,836	16%	100%
EAST	23,401	8,506	36%	2,247	10%	2,202	9%	7,005	30%	5,643	24%	100%



2 SOUTH ELEVATION
A3-2 3/32" = 1'-0"



1 EAST ELEVATION
A3-2 3/32" = 1'-0"

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KEY PLAN

BLAKE STATION SITE B

SOUTH AND EAST BUILDING ELEVATIONS

A3-2

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ELEVATION VIEW	TOTAL AREA	GLASS AREA	GLASS %	BRICK AREA	BRICK %	METAL PANEL AREA	METAL PANEL %	METAL TILE AREA	METAL TILE %	FIBER CEMENT SIDING AREA	FIBER CEMENT SIDING %	TOTAL %
NORTH	21,046	5,851	28%	4,861	23%	1,090	5%	6,545	31%	3,789	18%	100%
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EAST	23,401	8,506	36%	2,247	10%	2,202	9%	7,005	30%	5,643	24%	100%



2 NORTH ELEVATION
A3-3 3/32" = 1'-0"



1 WEST ELEVATION
A3-3 3/32" = 1'-0"

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BLAKE STATION SITE B

WEST AND NORTH
BUILDING ELEVATIONS

A3-3

WALL AREA = 1,565 SF
WINDOW AREA = 470 SF
TRANSPARENCY = 30%



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Minneapolis, MN 55415
p 612.339.5508 | f 612.339.5382
www.esgarch.com

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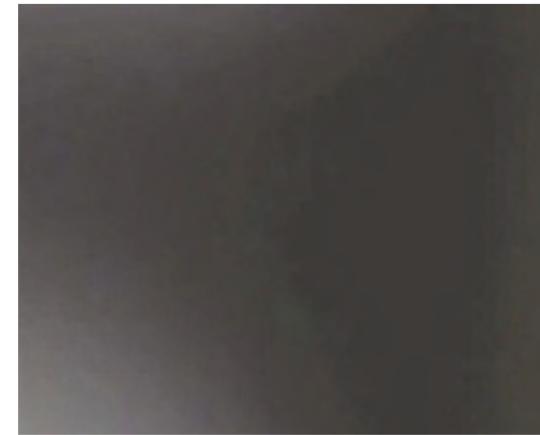
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METAL TILE



FIBER CEMENT LAP SIDING



METAL PANEL

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BLAKE STATION SITE B

EXTERIOR MATERIALS

A3-4



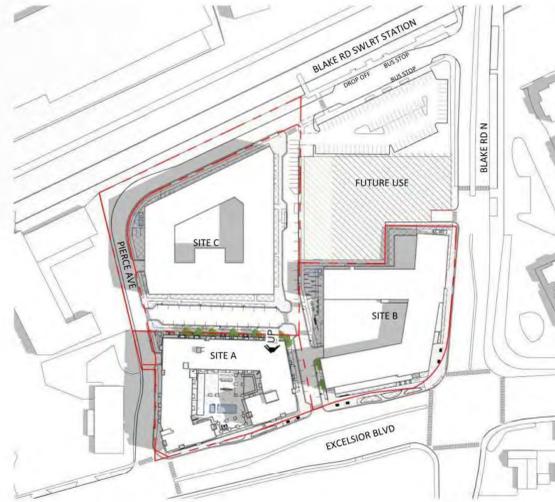
FACE BRICK - PRIMARY



FACE BRICK - ACCENT



SHADOW STUDY - MARCH 9 AM



SHADOW STUDY - JUNE 9 AM



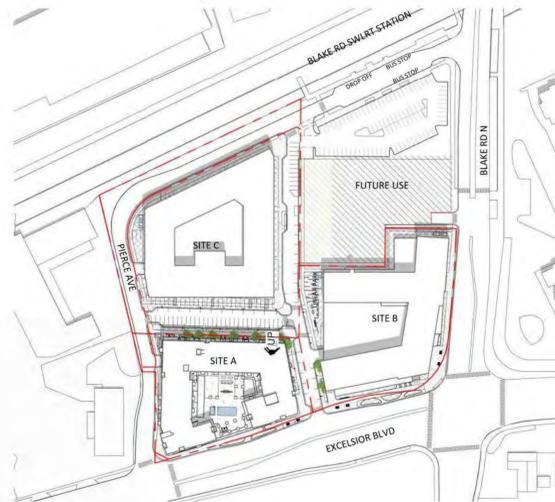
SHADOW STUDY - SEPTEMBER 9 AM



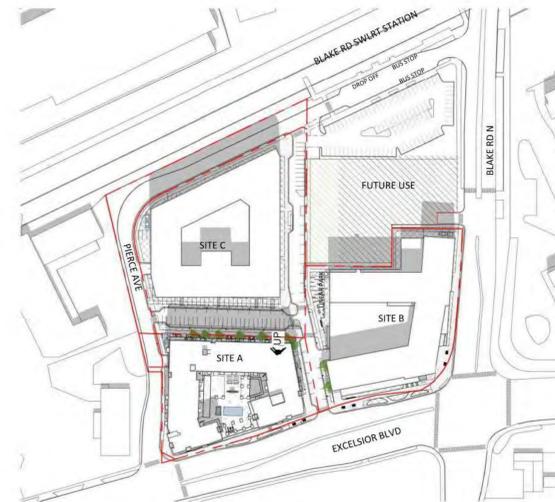
SHADOW STUDY - DECEMBER 9 AM



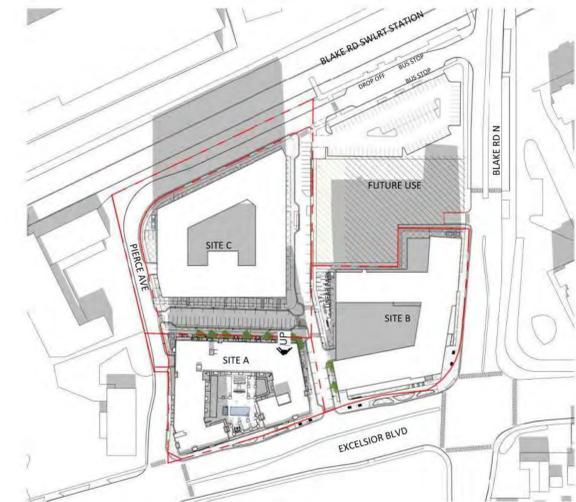
SHADOW STUDY - MARCH 12 PM



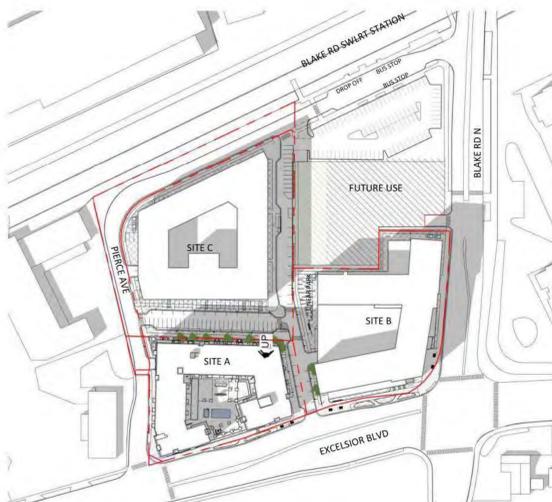
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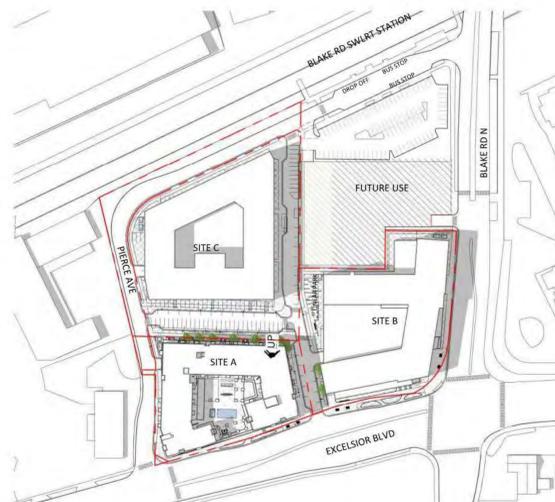
SHADOW STUDY - SEPTEMBER 12 PM



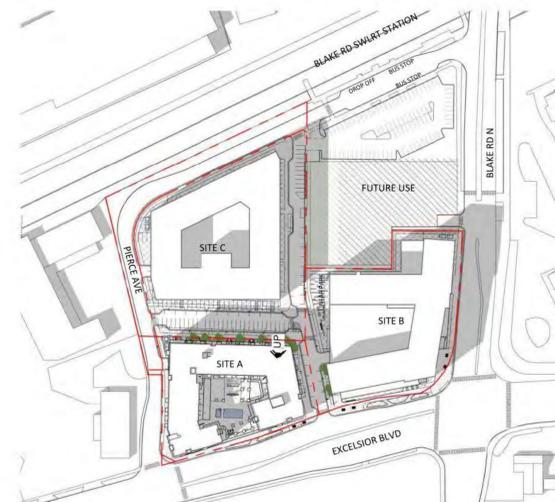
SHADOW STUDY - DECEMBER 12 PM



SHADOW STUDY - MARCH 3 PM



SHADOW STUDY - JUNE 3 PM



SHADOW STUDY - SEPTEMBER 3 PM



SHADOW STUDY - DECEMBER 3 PM

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BLAKE STATION SITE B

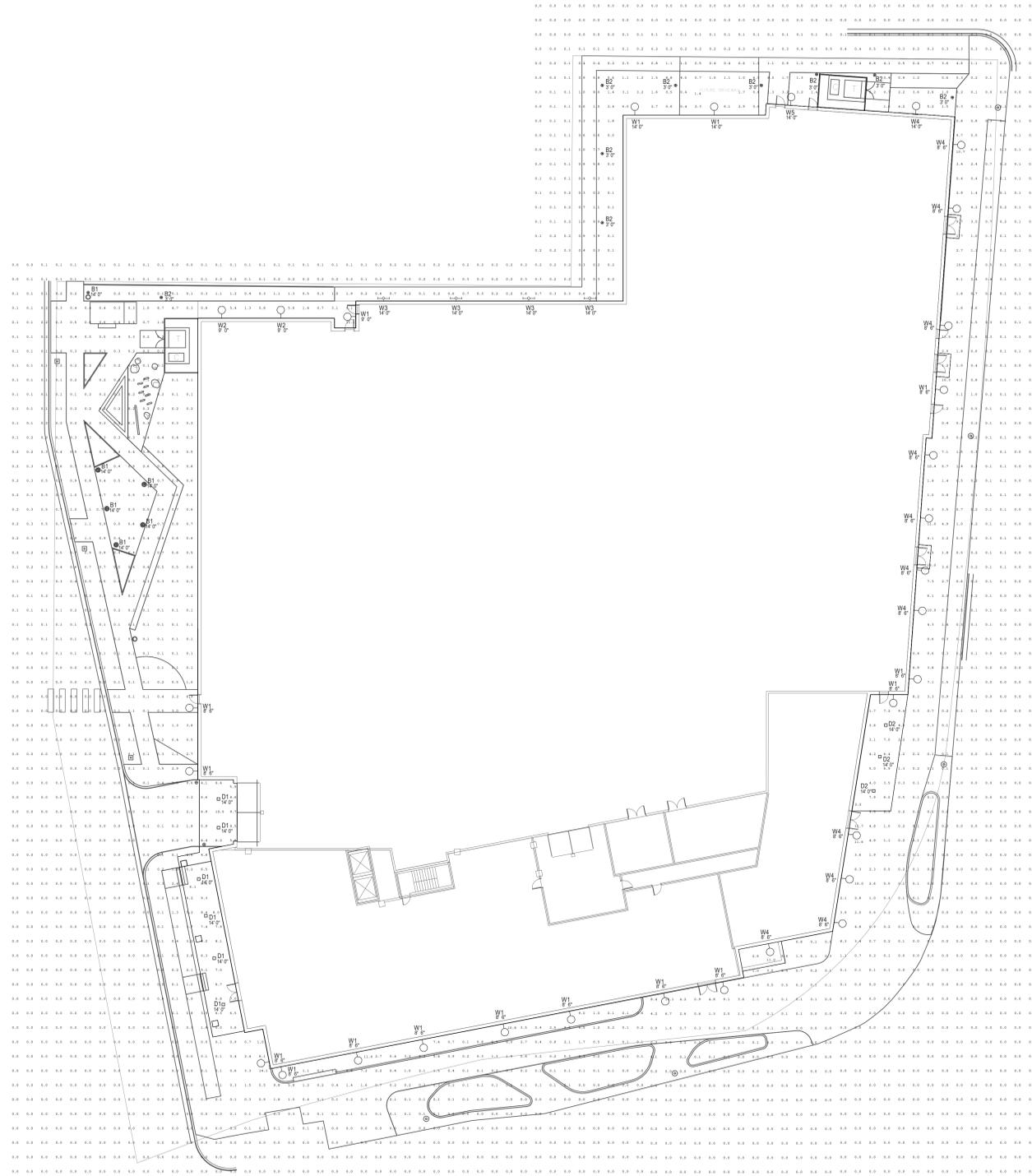
SHADOW STUDIES

A3-5

GENERAL NOTES:

- A. REFER TO SHEETS E0.2P, E0.3P, AND E0.4P FOR FIXTURE SCHEDULES, DETAILS, AND OUTSHEETS.
- B. LIGHTING LEVELS SHOWN ARE MAINTAINED. LIGHT LOSS FACTOR OF 0.80 WAS USED TO ACCOUNT FOR DIRT AND LUMEN DEPRECIATION.
- C. WALL SCONCE AND DOWNLIGHT MOUNTING HEIGHTS SHOWN ARE TO BOTTOM OF FIXTURE.
- D. BOLLARD MOUNTING HEIGHTS SHOWN ARE TO TOP OF FIXTURE.

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
East Property Line	Illuminance	FC	0.26	2.0	0.0	N/A	N/A
North Property Line	Illuminance	FC	0.24	1.7	0.1	1.44	17.00
Northeast Egress Path	Illuminance	FC	2.67	8.9	0.3	8.90	29.67
Northwest Egress Path	Illuminance	FC	3.27	10.1	0.3	10.10	33.67
Overall Site	Illuminance	FC	0.48	3.4	0.0	N/A	N/A
South Property Line	Illuminance	FC	0.14	1.6	0.0	N/A	N/A
West Property Line	Illuminance	FC	0.14	0.7	0.0	N/A	N/A



esg
ARCHITECTURE & DESIGN

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I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota

Signature

Typed or Printed Name

License # Date



emanuelson-podas
consulting engineers

Emanuelson-Podas, Inc.
7705 Bush Lake Road
Edina, MN 55439

952.930.0050 | www.epinc.com

**NOT FOR
CONSTRUCTION**

**LUA Submittal
02/18/2022**

ORIGINAL ISSUE:
02/18/22

REVISIONS:
No. Description Date

4452.0002
PROJECT NUMBER

MBS DRAWN BY JJA CHECKED BY

KEY PLAN



Blake Station Site B

ELECTRICAL SITE
PHOTOMETRIC

E0.1P

1 ELECTRICAL SITE PHOTOMETRIC
SCALE: 1"=20'-0"

6" LED Downlight LF6SL LENS 120V-277V 0-10V Dimming

DATE: _____ TYPE: _____
FIRM NAME: _____
PROJECT: D1

LifeFrame

Ordering Number: 01747
Maximum Ceiling Thickness 1 1/2"
For comparison to millimeters, multiply inches by 25.4
Not to Scale

APPLICATIONS:
The LF6SL is a 6" specification grade LED downlight that delivers high efficiency (90+) to allow color consistency, energy savings, and low maintenance costs. 30,000 hours minimum life expectancy in typical downlight applications.

HOUSING:
Designed to solve non-recession ceiling problems. The white, clear, or black housing is available in 100, 150, 200, 250, 300, 350, 400, 450, 500, 550, 600, 650, 700, 750, 800, 850, 900, 950, 1000, 1050, 1100, 1150, 1200, 1250, 1300, 1350, 1400, 1450, 1500, 1550, 1600, 1650, 1700, 1750, 1800, 1850, 1900, 1950, 2000, 2050, 2100, 2150, 2200, 2250, 2300, 2350, 2400, 2450, 2500, 2550, 2600, 2650, 2700, 2750, 2800, 2850, 2900, 2950, 3000, 3050, 3100, 3150, 3200, 3250, 3300, 3350, 3400, 3450, 3500, 3550, 3600, 3650, 3700, 3750, 3800, 3850, 3900, 3950, 4000, 4050, 4100, 4150, 4200, 4250, 4300, 4350, 4400, 4450, 4500, 4550, 4600, 4650, 4700, 4750, 4800, 4850, 4900, 4950, 5000, 5050, 5100, 5150, 5200, 5250, 5300, 5350, 5400, 5450, 5500, 5550, 5600, 5650, 5700, 5750, 5800, 5850, 5900, 5950, 6000, 6050, 6100, 6150, 6200, 6250, 6300, 6350, 6400, 6450, 6500, 6550, 6600, 6650, 6700, 6750, 6800, 6850, 6900, 6950, 7000, 7050, 7100, 7150, 7200, 7250, 7300, 7350, 7400, 7450, 7500, 7550, 7600, 7650, 7700, 7750, 7800, 7850, 7900, 7950, 8000, 8050, 8100, 8150, 8200, 8250, 8300, 8350, 8400, 8450, 8500, 8550, 8600, 8650, 8700, 8750, 8800, 8850, 8900, 8950, 9000, 9050, 9100, 9150, 9200, 9250, 9300, 9350, 9400, 9450, 9500, 9550, 9600, 9650, 9700, 9750, 9800, 9850, 9900, 9950, 10000, 10050, 10100, 10150, 10200, 10250, 10300, 10350, 10400, 10450, 10500, 10550, 10600, 10650, 10700, 10750, 10800, 10850, 10900, 10950, 11000, 11050, 11100, 11150, 11200, 11250, 11300, 11350, 11400, 11450, 11500, 11550, 11600, 11650, 11700, 11750, 11800, 11850, 11900, 11950, 12000, 12050, 12100, 12150, 12200, 12250, 12300, 12350, 12400, 12450, 12500, 12550, 12600, 12650, 12700, 12750, 12800, 12850, 12900, 12950, 13000, 13050, 13100, 13150, 13200, 13250, 13300, 13350, 13400, 13450, 13500, 13550, 13600, 13650, 13700, 13750, 13800, 13850, 13900, 13950, 14000, 14050, 14100, 14150, 14200, 14250, 14300, 14350, 14400, 14450, 14500, 14550, 14600, 14650, 14700, 14750, 14800, 14850, 14900, 14950, 15000, 15050, 15100, 15150, 15200, 15250, 15300, 15350, 15400, 15450, 15500, 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87050, 87100, 87150, 87200, 87250, 87300, 87350, 87400, 87450, 87500, 87550, 87600, 87650, 87700, 87750, 87800, 87850, 879

Styk Exterior Wall - Bracket

SEW12145 2 in

JOB NAME:
TYPE: **W3**
NOTES:

DESCRIPTION

Styk was built to provide the most lumens with the smallest luminaire package, while optimizing life. Not limited to decorative lighting, effective functional lighting is provided through the optimal light control in this minimal package. It's three optical offerings enable tremendous flexibility including wall washing, wall grazing, and asymmetric lighting solutions. It is excellent at lighting signs and facades. The family features wall, ceiling, and pendant models in a variety of styles, clean mounting options. Available in 1' to 8' lengths, Styk can be mounted individually or configured in runs.

FEATURES & BENEFITS

- A forward throw optic, for even wall wash illumination, is standard
- Symmetrical optic options are available for more volumetric illumination
- Lamp body can be rotated up to 330° and locks into position for precise fixture alignment
- Up to 1,450 lumens per foot delivered (with Forward Throw distribution)
- Anodized finish provides durable corrosion protection
- All visible fasteners are flush mounted, providing a clean design
- Handcrafted in USA

SPECIFICATIONS

- LIGHT SOURCE:** IP66 white LED light engine
- CRB:** 80+ (contact factory for 90+)
- LUMEN MAINTENANCE:** L70 => >50,000 Hrs.
- EFFICACY:** 102 lm/W delivered (with Forward Throw distribution)
- CCT:** 3000K, 3500K, or 4000K
- VOLTAGE:** 120-277V standard
- DRIVER:** Includes remote damp listed Class 2 power supply and wet location enclosure. 36" lead length standard; black power cord standard unless otherwise specified. Continuous runs are supplied with one power supply per fixture. Max distance to the driver (including OAP) is: #18 AWG = 50', #16 AWG = 75', #14



designed for
LED **WET** **EMR**

SPI LIGHTING PROJECT DETAILS

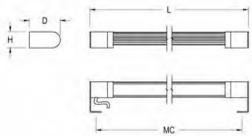
P:263.242.1420 | S:PIteam@spilighting.com | Last Revised: 9/10/2021 | Design Rights Reserved | SEW12145 | 1 of 6

1 **FIXTURE TYPE W3**
NO SCALE

AWG = 100'. For extended distances, contact factory.

- DIMMING:** 0-10V controls standard to 1%
- EMERGENCY:** Emergency battery remote optional
- INTEGRATED SURGE PROTECTION:** LED components are protected against minor surge events
- CONSTRUCTION:** Extruded aluminum construction provides durable protection for internal components and is recyclable
- FINISH:** Housing available in anodized finishes only. End caps and mounting components painted to match.
- MODIFICATIONS:** Consult factory for all modification requests, including RGB and static LED colors
- APPROVALS:** ETL listed to UL standards (US and Canada) for use in wet locations

DIMENSIONS



1FT	L	H	D	MC
36.4 cm	1.6 m	3.9	38.6 cm	
43.7 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 5 lb (2 kg)				
2FT	L	H	D	MC
72.8 cm	1.6 m	3.9	38.6 cm	
71.6 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 5 lb (2 kg)				
3FT	L	H	D	MC
109.2 cm	1.6 m	3.9	38.6 cm	
101.3 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 5 lb (2 kg)				
4FT	L	H	D	MC
145.6 cm	1.6 m	3.9	38.6 cm	
138.0 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 5 lb (2 kg)				
5FT	L	H	D	MC
182.0 cm	1.6 m	3.9	38.6 cm	
181 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 10 lb (5 kg)				
6FT	L	H	D	MC
218.4 cm	1.6 m	3.9	38.6 cm	
194.3 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 10 lb (5 kg)				
7FT	L	H	D	MC
254.8 cm	1.6 m	3.9	38.6 cm	
251.2 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 10 lb (5 kg)				
8FT	L	H	D	MC
291.2 cm	1.6 m	3.9	38.6 cm	
291 cm	4.1 cm	7.6 cm		
Mounting Weight: Approximate: 10 lb (5 kg)				

SPI LIGHTING P:263.242.1420 | S:PIteam@spilighting.com | Last Revised: 9/10/2021 | Design Rights Reserved | SEW12145 | 2/21/21

2 **FIXTURE TYPE W4**
NO SCALE

CONFIGURATOR

To configure your spec sheet online, go to www.spilighting.com/SEW12145. Not all options are available in all configurations; consult factory for details.

Required Field *

Category: SEW12145 | Light Source: A | Primary Finish: B | Voltage: C | Lamp Options: D | Mounting: E | Linear Run: F | Emergency: G | Optical Distribution: H | Options: I

A - LIGHT SOURCE *

To ensure color consistency, SPI uses precise bin selection and strict quality processes to maintain a 3-step (MacAdam) SDCM on all white LED lampings. Published LED luminaire wattages are calculated using a typical power supply efficiency of 88%, exact wattages may vary based on application. Alternative wattages available upon request. Delivered Lumens shown below are for Forward Throw distribution.

1 FOOT NOMINAL FIXTURE

- 1FT-L4W | White 4W LED Light Engine | Delivered Lumens: 407
- 1FT-L7W | White 7W LED Light Engine | Delivered Lumens: 712
- 1FT-L14W | White 14W LED Light Engine | Delivered Lumens: 1424

2 FOOT NOMINAL FIXTURE

- 2FT-L8W | White 8W LED Light Engine | Delivered Lumens: 814
- 2FT-L14W | White 14W LED Light Engine | Delivered Lumens: 1424
- 2FT-L28W | White 28W LED Light Engine | Delivered Lumens: 2848

3 FOOT NOMINAL FIXTURE

- 3FT-L12W | White 12W LED Light Engine | Delivered Lumens: 1229
- 3FT-L21W | White 21W LED Light Engine | Delivered Lumens: 2136
- 3FT-L42W | White 42W LED Light Engine | Delivered Lumens: 4271

4 FOOT NOMINAL FIXTURE

- 4FT-L16W | White 16W LED Light Engine | Delivered Lumens: 1627
- 4FT-L28W | White 28W LED Light Engine | Delivered Lumens: 2848
- 4FT-L56W | White 56W LED Light Engine | Delivered Lumens: 5695

5 FOOT NOMINAL FIXTURE

- 5FT-L20W | White 20W LED Light Engine | Delivered Lumens: 2034
- 5FT-L35W | White 35W LED Light Engine | Delivered Lumens: 3560
- 5FT-L71W | White 71W LED Light Engine | Delivered Lumens: 7221

6 FOOT NOMINAL FIXTURE

- 6FT-L24W | White 24W LED Light Engine | Delivered Lumens: 2441
- 6FT-L42W | White 42W LED Light Engine | Delivered Lumens: 4271
- 6FT-L85W | White 85W LED Light Engine | Delivered Lumens: 8545

8 FOOT NOMINAL FIXTURE

- 8FT-L32W | White 32W LED Light Engine | Delivered Lumens: 3254
- 8FT-L56W | White 56W LED Light Engine | Delivered Lumens: 5695
- 8FT-L113W | White 113W LED Light Engine | Delivered Lumens: 11492

See last page for finish options

SPI LIGHTING P:263.242.1420 | S:PIteam@spilighting.com | Last Revised: 9/10/2021 | Design Rights Reserved | SEW12145 | 3/21/21

3 **FIXTURE TYPE W5**
NO SCALE



500 Washington Avenue South, Suite 1080
Minneapolis, MN 55415
p 612.339.5508 | f 612.339.5382
www.esgarch.com

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed professional engineer under the laws of the State of Minnesota

Signature
Typed or Printed Name
License # Date

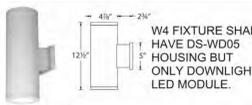


Emanulson-Podas, Inc.
7705 Bush Lake Road
Edina, MN 55439
952.930.0050 | www.epinc.com

NOT FOR CONSTRUCTION

TUBE ARCHITECTURAL DS-WD05 WAC LIGHTING

LED Wall Mounts



W4 FIXTURE SHALL HAVE DS-WD05 HOUSING BUT ONLY DOWNLIGHT LED MODULE.

Fixture Type: **W4**

Catalog Number: []

Project: []

Location: []

PRODUCT DESCRIPTION

The latest energy efficient LED technology in an appealing cylindrical profile delivers accent and wall wash lighting. Comes in various light distribution and beam angle options.

FEATURES

- High performance exterior rated LED wall mount light
- Fixture can install upside down to alter light distribution
- Solid aluminum construction
- 5 year warranty

ORDERING NUMBER

Input:	Dimming:	Universal voltage 120V - 277VAC, 50/60Hz
Electronic low voltage (ELV): 100% - 5%	0-10V; 100% - 1%	
Light Source: High output 3 Step Mac, Adam Ellipse COB	Finish: Electrostatically powder coated, white, black, bronze and graphite IP65 rated, ETL & cETL wet location listed	Standard: Title 24, IMB 2016 Compliant
Operating Temp: -13F to 122F (-25C to 50C)		

Reference Output	Beam Angle	Color Temp	CRB	LMEN	CECF	Efficacy (lm/w)	Light Distribution	Finish
9275	2700K	90	1825 x 2	10050 x 2	73 x 2	2700K	S	180°
275	2700K	85	2180 x 2	12057 x 2	88 x 2	2700K	S	180°
9305	3000K	90	1885 x 2	10627 x 2	78 x 2	3000K	S	180°
305	3000K	85	2300 x 2	12644 x 2	92 x 2	3000K	S	180°
355	3500K	85	2350 x 2	12897 x 2	94 x 2	3500K	S	180°
405	4000K	85	2375 x 2	13062 x 2	95 x 2	4000K	S	180°
9275	2700K	90	1810 x 2	5280 x 2	72 x 2	2700K	N	25°
275	2700K	85	2170 x 2	6353 x 2	87 x 2	2700K	N	25°
9305	3000K	90	1885 x 2	5478 x 2	75 x 2	3000K	N	25°
305	3000K	85	2275 x 2	6643 x 2	91 x 2	3000K	N	25°
355	3500K	85	2330 x 2	6797 x 2	93 x 2	3500K	N	25°
405	4000K	85	2350 x 2	6863 x 2	94 x 2	4000K	N	25°
9275	2700K	90	1800 x 2	4515 x 2	76 x 2	2700K	F	33°
275	2700K	85	2280 x 2	5477 x 2	91 x 2	2700K	F	33°
9305	3000K	90	1870 x 2	4685 x 2	79 x 2	3000K	F	33°
305	3000K	85	2390 x 2	5887 x 2	96 x 2	3000K	F	33°
355	3500K	85	2445 x 2	5972 x 2	98 x 2	3500K	F	33°
405	4000K	85	2470 x 2	5870 x 2	99 x 2	4000K	F	33°
927A	2700K	90	2000 x 2	80 x 2	80 x 2	2700K	F	AWAY
27A	2700K	85	2400 x 2	96 x 2	96 x 2	2700K	F	AWAY
930A	3000K	90	2075 x 2	82 x 2	82 x 2	3000K	F	AWAY
30A	3000K	85	2525 x 2	101 x 2	101 x 2	3000K	F	AWAY
35A	3500K	85	2575 x 2	107 x 2	107 x 2	3500K	F	AWAY
40A	4000K	85	2600 x 2	104 x 2	104 x 2	4000K	F	AWAY
927B	2700K	90	2000 x 2	80 x 2	80 x 2	2700K	F	TOWARDS
27B	2700K	85	2400 x 2	96 x 2	96 x 2	2700K	F	TOWARDS
930B	3000K	90	2075 x 2	82 x 2	82 x 2	3000K	F	TOWARDS
30B	3000K	85	2525 x 2	101 x 2	101 x 2	3000K	F	TOWARDS
35B	3500K	85	2575 x 2	107 x 2	107 x 2	3500K	F	TOWARDS
40B	4000K	85	2600 x 2	104 x 2	104 x 2	4000K	F	TOWARDS
927C	2700K	90	2000 x 2	80 x 2	80 x 2	2700K	F	ONE SIDE AWAY FROM WALL
27C	2700K	85	2400 x 2	96 x 2	96 x 2	2700K	F	ONE SIDE AWAY FROM WALL
930C	3000K	90	2075 x 2	82 x 2	82 x 2	3000K	F	ONE SIDE AWAY FROM WALL
30C	3000K	85	2525 x 2	101 x 2	101 x 2	3000K	F	ONE SIDE AWAY FROM WALL
35C	3500K	85	2575 x 2	107 x 2	107 x 2	3500K	F	ONE SIDE AWAY FROM WALL
40C	4000K	85	2600 x 2	104 x 2	104 x 2	4000K	F	ONE SIDE AWAY FROM WALL

DS-WD05 Example: DS-WD05-F930A-WT Reference output shows 25W output. Multiply by 0.7 to determine output for 17W combination.

DS-WD0534 Example: DS-WD0534-F930A-WT Reference output shows 70W output. Multiply by 0.7 to determine output for 17W combination.

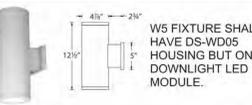
WAC Lighting retains the right to modify the design of our products at any time as part of the company's continuous improvement program. SEPT 2019

wacighting.com Phone (800) 526-2588 Fax (800) 526-2585
Headquarters/Eastern Distribution Center 44 Harbor Park Drive Port Washington, NY 11050
Central Distribution Center 1600 Distribution Ct Lithia Springs, GA 30122
Western Distribution Center 1750 Archibald Avenue Ontario, CA 91760

2 **FIXTURE TYPE W4**
NO SCALE

TUBE ARCHITECTURAL DS-WD05 WAC LIGHTING

LED Wall Mounts



W5 FIXTURE SHALL HAVE DS-WD05 HOUSING BUT ONLY DOWNLIGHT LED MODULE.

Fixture Type: **W5**

Catalog Number: []

Project: []

Location: []

PRODUCT DESCRIPTION

The latest energy efficient LED technology in an appealing cylindrical profile delivers accent and wall wash lighting. Comes in various light distribution and beam angle options.

FEATURES

- High performance exterior rated LED wall mount light
- Fixture can install upside down to alter light distribution
- Solid aluminum construction
- 5 year warranty

ORDERING NUMBER

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35A	3500K	85	2575 x 2	107 x 2	107 x 2	3500K	F	AWAY
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30B	3000K	85	2525 x 2	101 x 2	101 x 2	3000K	F	TOWARDS
35B	3500K	85	2575 x 2	107 x 2	107 x 2	3500K		

SUSTAINABLE DESIGN — SITE AND LANDSCAPE DESIGN

- **Transit-oriented development** location adjacent to LRT station. Light rail/bus pass discount program offered to encourage use of mass transit.
- **Native and adaptive plant species** selected for their ability to provide enhanced streetscapes, additional habitat and stormwater management strategies
- **Removal of more than 45,890 sq ft of existing surface parking areas** and replaced with new buildings and **38,352 sq ft area of landscape area** consisting of native and adaptive tree and plant species
- **Smart irrigation systems** (rain/soil moistures, drip irrigation, highly efficient controllers/sprayheads)
- Connecting to existing **bike trails** and providing internal bike storage rooms
- Advocate for **mirco-mobility**: shared bike or scooter parking stations including maps of local trails
- **Porous pavement** integrated into streetscape improvements
- On-site **community gardens** available for resident use



SUSTAINABLE DESIGN — STORM WATER MANAGEMENT

- **Dramatic improvements over current site conditions** - currently 100% impervious surfaces that sheet drain to streets untreated; future pervious area equal to 12% of the total site area (38,352 sq ft)
- Future water **run-off collected** in catch basins
- **Sub-surface system** in StormTech isolator rows
- **Treat 2” of run-off** from the site’s impervious surfaces via a treatment train of above-ground features, pipe storage system, and JellyFish type treatment structure.
- Storage capacity designed to **hold back storm events** and discharge run-off at rates that are below those of the existing conditions



SUSTAINABLE DESIGN — BUILDING DESIGN

ENERGY EFFICIENCY & SAVINGS

- High R-Value building envelope — roof & walls
- High solar reflectance index (SRI) for the roof
- Low-E insulated glazing
- Tightly sealed building envelope to reduce leakage and inefficiencies
- High-Efficiency HVAC system with individual controls for residents
- Project is enrolled in the Xcel Energy Design Assistnace (EDA) program to evaluate and select efficient mechanical systems and optimize the building enclosure
- Water Sense plumbing fixtures and Energy Star rated appliances
- Induction cooktops for 20% of units
- LED fixtures with occupancy sensors
- Programmable or SMART thermostats
- Electrical vehicle charging stations (20 in Building B)
- Building structure and electrical system are designed to be Solar-Ready for future photovoltaic panels



SUSTAINABLE DESIGN — OPERATIONS

HEALTH & WELLNESS

- Designed to encourage **physical activity** - primary and secondary pedestrian-oriented entrance, each with a convenient stairwell to encourage physical activity; bike racks and storage available with site improvements that include trail along Excelsior Blvd
- **Clean air** with at least MERV 8 air filters. Apartments will be supplied with outside air from individual packaged heatingcooling units dedicated to each space.
- Access to **daylight** in units and amenity spaces - Vertically oriented windows allow daylight to reach deep into dwellings and common areas
- **Low VOC** materials and paints
- Convenient common area **hand sanitizers and hand washing** locations
- **Green janitorial practices** - partnering with companies that share green janitorial initiatives
- **Operable windows and balcony doors** ensure access to natural ventilation and connection to the outdoors
- **Ceiling Fans** in bedrooms = reduced cooling needs
- Fitness centers designed with **state-of-the-art equipment** and input from internal health & wellness director



SUSTAINABLE DESIGN — OPERATIONS

EFFICIENT MANAGEMENT PRACTICES

- **Recycling collection** at each trash shoot and separate bins within amenity spaces, making recycling more convenient and including clear instructions for recycling materials
- Looking to partner with **givebackbox.com** onsite
- **Composting bins** provided for resident use
- **Reusable water bottles** provided at move-in and refill stations throughout the property
- **Virtual flipbook** vs. paper brochures for leasing to reduce paper waste
- **Smart keyless entry** (eliminates the need for replacement keys/fobs)
- Rent payments encouraged to be electronic by implementing a check processing fee
- **Healthy building design practices** and investigate Healthy Building certification



CITY OF HOPKINS
Hennepin County, Minnesota

PLANNING & ZONING COMMISSION RESOLUTION 2022-01

**A RESOLUTION RECOMMENDING THE CITY COUNCIL APPROVE AN AMENDMENT
TO THE BLAKE ROAD STATION PLANNED UNIT DEVELOPMENT AGREEMENT TO
INCORPORATE SITE B (19-117-21-42-0046), SUBJECT TO CONDITIONS**

WHEREAS, the applicant, Bryan Farquhar of TF Hopkins, LLC (Trilogy Real Estate Group), initiated an application for an amendment to the Blake Road Station PUD agreement to incorporate Site B into the original PUD agreement; and

WHEREAS, the subject property is legally described as Lot 3, Hopkins Commerce Addition with PID 19-117-21-42-0046; and

WHEREAS, the procedural history of the application is as follows:

1. That the above stated application was initiated by the applicant on February 18, 2022; and,
2. That the Hopkins Planning & Zoning Commission, pursuant to published and mailed notice, held a public hearing on the application and reviewed such application on March 22, 2022: all persons present were given an opportunity to be heard; and,
3. That written comments and analysis of City staff were considered; and,

WHEREAS, staff recommended approval of the above stated application based on the findings outlined in the staff report dated March 22, 2022; and

NOW, THEREFORE, BE IT RESOLVED that the Planning & Zoning Commission of the City of Hopkins hereby recommends the City Council of the City of Hopkins approve an amendment to the Blake Road Station Planned Unit Development agreement to incorporate Site B with PID 19-117-21-42-0046, subject to the conditions listed below.

1. Execution of an amendment to the Blake Road Station Planned Unit Development (PUD) Agreement in a form acceptable to the City Attorney.
2. Approval of the associated Site Plan for Blake Road Station Site B and compliance with all associated conditions.

Adopted this 22nd day of March, 2022.

Samuel Stiele, Chair

CITY OF HOPKINS
Hennepin County, Minnesota

PLANNING & ZONING COMMISSION RESOLUTION 2022-02

**A RESOLUTION RECOMMENDING THE CITY COUNCIL APPROVE THE SITE PLAN
FOR BLAKE ROAD STATION SITE (19-117-21-42-0046), SUBJECT TO CONDITIONS**

WHEREAS, the applicant, Bryan Farquhar of TF Hopkins, LLC (Trilogy Real Estate Group), initiated a site plan review application for Blake Road Station PUD Site B; and

WHEREAS, the subject property is legally described as Lot 3, Hopkins Commerce Addition with PID 19-117-21-42-0046; and

WHEREAS, the procedural history of the application is as follows:

1. That the above stated application was initiated by the applicant on February 18, 2022; and,
2. That the Hopkins Planning & Zoning Commission, pursuant to published and mailed notice, held a public hearing on the application and reviewed such application on March 22, 2022: all persons present were given an opportunity to be heard; and,
3. That written comments and analysis of City staff were considered; and,

WHEREAS, staff recommended approval of the above stated application based on the findings outlined in the staff report dated March 22, 2022; and

NOW, THEREFORE, BE IT RESOLVED that the Planning & Zoning Commission of the City of Hopkins hereby recommends the City Council of the City of Hopkins approve the site plan for Blake Road Station Site B with PID 19-117-21-42-0046, subject to the conditions listed below.

1. Conformance with all applicable standards of the Mixed Use District and terms of the Blake Road Station Planned Unit Development (PUD) Agreement.
2. Plan revisions to reallocate the parking for Site B so that 289 stalls are assigned to residential uses and 50 stalls are assigned to commercial uses to ensure the commercial parking complies with the 1 stall per 200 square feet commercial parking standard.
3. Plan revisions to the proposed residential bicycle parking to show at least 138 long-term bicycle parking stalls within the building and at least 13 short-term stalls outside the building. Any bicycle parking located within the public right-of-way must be approved by the City and or Hennepin County.
4. Plan revisions to the proposed commercial bicycle parking to provide at least 5 long-term commercial bicycle parking stalls inside the building in close proximity to the commercial spaces. The balance of the 15 commercial bicycle parking stalls may be located outside the building close to the entrances to the commercial space. Any bicycle parking located within the public right-of-way must be approved by the City and or Hennepin County.

5. Plan revisions to include architectural features that will make the building entrance along Blake Road more noticeable to pedestrians.
6. Plan revisions to provide transparency on the first floor of at least 60 percent along the south, east and west sides of the building and at least 50 percent along the north side of the building. The applicant shall also submit information demonstrating at least 30 percent fenestration for windows above the first floor on all sides of the building.
7. Plan revisions to include a building cap architectural feature on all sides of the building (parapet with a horizontal shadow line, lintel or other architectural technique acceptable to the City Planner).
8. Provision of a detailed pedestrian and streetscape plan demonstrating consistency with those features used on Site A.
9. Provision of a landscape and pedestrian/streetscape letter of credit in a form acceptable to the City Attorney for 125% of the value of all improvements. The City will hold the letter of credit until all pedestrian/streetscape improvements have been install and all plantings have survived at least one growing season.
10. The applicant plans to store trash containers within the underground parking garage. The applicant shall designate a specific location within the Level 1 parking garage and an on-site location for trash pickup. Trash pick-up shall not occur within the public right-of-way or any street surface.
11. Submission and approval of a detailed lighting and photometric plan for Site B prior to action on this item by the City Council. This plan shall include both pedestrian-scale street lights (no greater than 12' in height) as well as all building exterior lighting, light fixture spec sheets and photometric data demonstrating equal to or less than 1.0 lumens at any non-residential property line or 0.5 lumens at any residential property line.
12. The applicant shall demonstrate all equipment will be screened in conformance with all zoning requirements.
13. Conformance with all requirements of Hennepin County and the Metropolitan Council.
14. Conformance with all requirements of the City Engineer.
15. Approval of all necessary permits from the Building, Engineering and Fire Departments.
16. Approval of the development by the Minnehaha Creek Watershed District and conforms with all related conditions.
17. Payment of all applicable development fees including, but not limited to SAC, park dedication and City Attorney fees.
18. The applicant has indicated their intent to subdivide the properties as part of the future development of Sites C. The applicant has requested and the City has agreed to lock in the 2021 park dedication fees (\$3,000/unit and 5% of the fair market value of the commercial land as estimated by the county assessor) and apply them to the development on all 3 sites as part of any future subdivision of any of the 3 parcels.

19. Execution of an amendment to the Blake Road Station Planned Unit Development (PUD) Agreement in a form acceptable to the City Attorney.

Adopted this 22nd day of March, 2022.

Samuel Stiele, Chair